



SUPPORTING INFORMATION

for respective items considered at

General Meeting

20 August 2019

SUPPORTING INFORMATION

Ref: [A18705408](#)

The following list of supporting information is provided for:

ITEM 1.3

NEW DEFINED AREA - INCENTIVISING INFILL DEVELOPMENT - REGIONAL

#1 Map 4 - Arana Hills Centre Incentives Area

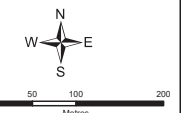
ITEM 1.3 NEW DEFINED AREA - INCENTIVISING INFILL DEVELOPMENT - REGIONAL - A18705369 (Cont.)

#1 Map 4 - Arana Hills Centre Incentives Area

MAP4 - ARANA HILLS CENTRE INCENTIVES AREA



- Legend**
- Incentive Area
 - Railway Station
 - Rail line
 - Planning Scheme Zone**
 - Centre



SCALE (A3) 1:5,000
15 August 2019
Ref: 1701-12-CA

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SUPPORTING INFORMATION

Ref: [A18917245](#) & [A18917246](#)

The following list of supporting information is provided for:

ITEM 2.1

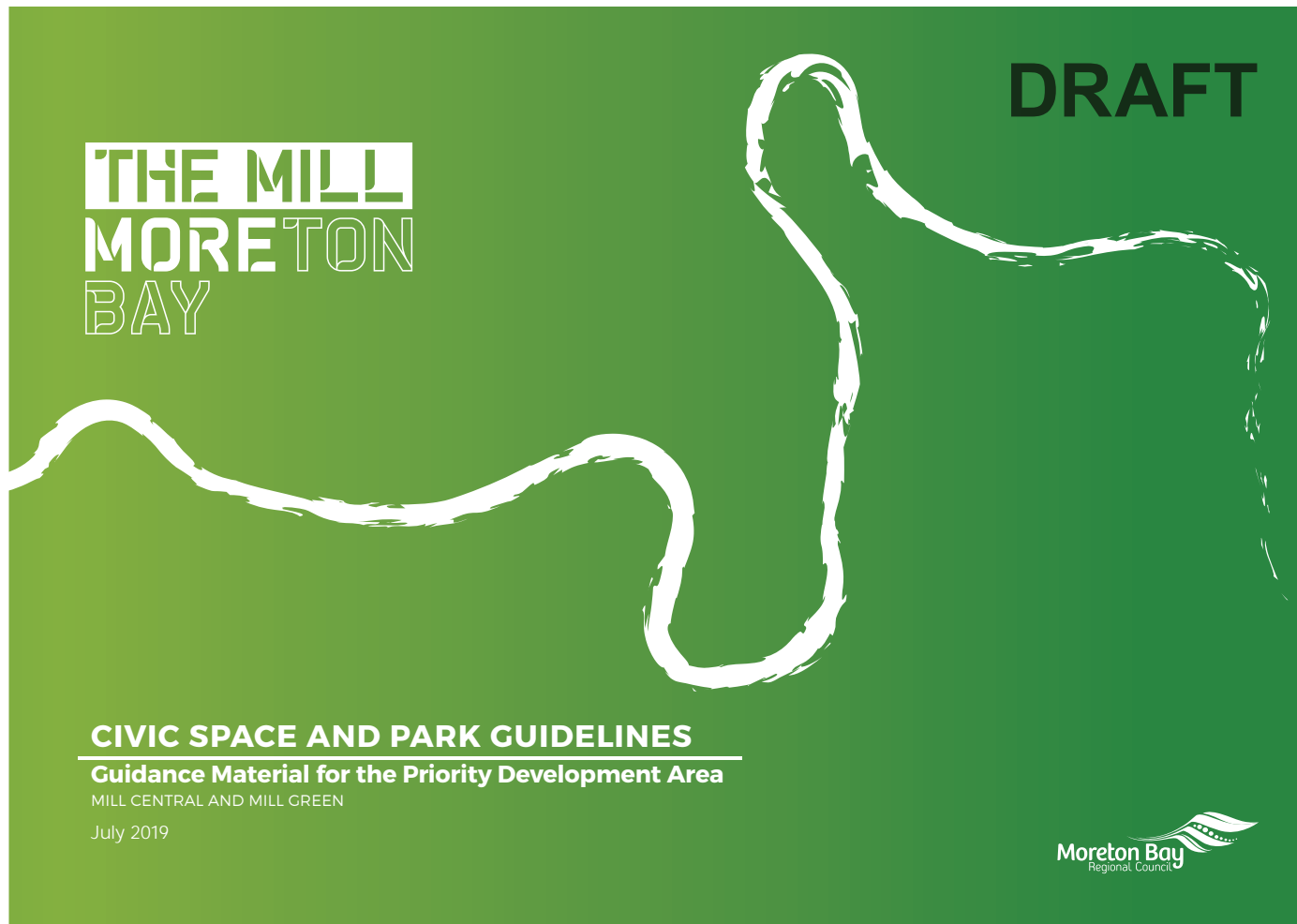
PROPOSED THE MILL AT MORETON BAY GUIDANCE MATERIAL: DRAFT STREETScape GUIDELINES AND DRAFT CIVIC SPACE AND PARK GUIDELINES AND PUBLIC CONSULTATION - DIVISION 7

#1 Civic Space and Park Guidelines

#2 Streetscape Guidelines

ITEM 2.1 PROPOSED THE MILL AT MORETON BAY GUIDANCE MATERIAL: DRAFT STREETScape GUIDELINES AND DRAFT CIVIC SPACE AND PARK GUIDELINES AND PUBLIC CONSULTATION - DIVISION 7 - A18894526 (Cont.)

#1 Civic Space and Park Guidelines



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**The Mill at Moreton Bay
Civic Space and Park Guidelines**

Revision 4.0
Issued 2 August 2019

Report Authors and Contributors

Author Moreton Bay Regional Council (Strategic Planning and Economic Development)
Contributor



Reviewed by the Mill Design Review Panel

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Artist's Impression 1: Key Civic Space - formal event crowd gathering space

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1. INTRODUCTION

The Mill Priority Development Area (PDA) aims to be a world-class destination, and as such its design and planning must be exemplary.

The Civic Space and Park Guidelines have been produced to assist in the delivery of The Mill at Moreton Bay Priority Development Area (PDA) vision for an active and embellished public realm, and high quality landscape and urban design outcomes.

These guidelines direct the desired character and experiential qualities of civic spaces and parks as places that are distinctive, engaging, comfortable, adaptive to changing user needs, connect people to living systems, and provide authentic connections to the past. The wildlife and conservation areas adjacent the parks, in *Mill Green*, have been significantly considered, but are not the focus of this guideline.

They assist the design, planning and assessment of civic spaces and parks in *Place 1 - Mill Central* and *Place 5 - Mill Green* in the PDA, and provide the designer with a clear indication of the outcomes that are desired.

These guidelines will be utilised by Moreton Bay Regional Council (MBRC), as Minister Economic Development Queensland (MEDQ) delegate, in the assessment of civic space and park proposals in *Mill Central* and *Mill Green*, and the Design Review Panel (DRP) in its role of providing specific design advice.

These guidelines also provide clarity to investors and the community on the civic spaces and parks expected in *Mill Central* and *Mill Green*.

The standards set out in these guidelines are intended to provide a degree of certainty and clarity of interpretation of the *Development Scheme*, however, as guidelines, they should also be afforded some flexibility in application. Proponents may therefore propose innovative and alternative solutions that demonstrate achievement of the PDA vision, relevant structural elements, PDA-wide criteria and place provisions in the *Development Scheme*.

These guidelines are an evolving document and may be revised, from time to time, to reflect new findings or recommendations made by further planning work for the PDA and on-going review of development outcomes achieved across *Mill Central* and *Mill Green*.

The Mill Civic Space and Park Guidelines are a companion to The Mill Streetscape Guidelines and other future Mill Guidelines.

RELATIONSHIP TO THE DEVELOPMENT SCHEME

The Mill Civic Space and Park Guidelines are 'guidance material' and are to be taken into account in the preparation and assessment of PDA development applications in accordance with sections 2.1.3, 2.2.2 and 2.6 of the *Development Scheme*.

These guidelines are required by the *Development Scheme's* Implementation Strategy to support the achievement of the vision for the Mill. The relevant Implementation Strategy action is:

4.6 Action 2. Develop additional guidance material as needed to assist in communicating the criteria within the development scheme (for example street profiles and cross sections).

As per the Implementation Strategy, these guidelines will assist in communicating the *Development Scheme* criteria.

These guidelines further refine specific criteria as per **2. Development Scheme Criteria** in this guideline (page 2).

RELATIONSHIP TO THE MORETON BAY REGIONAL COUNCIL PLANNING SCHEME

The Moreton Bay Regional Council Planning Scheme is also 'guidance material' and where relevant is taken into account in the preparation and assessment of PDA development applications (refer to section 2.2.12 of the *Development Scheme*).

Where these guidelines contain variations to aspects of the planning scheme, these guidelines prevail.

These guidelines prevail over the following public realm related outcomes, which the Moreton Bay Regional Council Planning Scheme also address:

- open space and civic space recreation; opportunities and facilities; and
- open space and civic space description and level of embellishment guide.

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2. DEVELOPMENT SCHEME CRITERIA

These guidelines provide further detail to specific PDA-wide criteria and Place Provision criteria in the *Development Scheme*. The following is a list of the relevant *Development Scheme* criteria and what part of this guideline provides further guidance that would satisfy and deliver what is required.

Table 1: Development Scheme Criteria and its guidance in this document

PDA-wide Criteria	Civic Space and Park guidance	PDA-wide Criteria	Civic Space and Park guidance
<p>2.5.2 Urban Design</p> <p>Development delivers high quality urban design by ensuring the form, type and arrangement of buildings, structures, streets and public spaces:</p> <ol style="list-style-type: none"> create an appealing, active, inclusive and vibrant urban environment facilitate a diverse range of uses, activities and experiences consistent with the PDA's intended role as a major university and mixed-use place, district centre, residential community and innovation hub delivers exemplary hard and soft streetscape, utilising sub-tropical design and water sensitive urban design principles, that soften and enhance the physical and visual amenity of the locality 	<p>The function and roles of each Civic Space and Park, as per section 3. Types and Functions, contribute to an appealing, active and vibrant urban environment.</p> <p>The guide and typical plans in section 4. Guide and Typical Plans illustrate an expectation of the uses, activities and experiences expected to achieve this criteria.</p> <p>Each park typology has a hardscape and softscape approach that creates its identity and fulfils its function.</p> <p>Hardscape, Softscape and Water Sensitive Urban Design guidance is provided in each Civic Space and Park in section 4. Guide and Typical Plans.</p>	<ol style="list-style-type: none"> creates a diversity of spaces that allow for equitable access for all users to a range of different experiences and uses, including large-scale events and recreation integrates public art that is appropriate to the origins, history and character of the area 	<p>A variety and network of civic spaces and parks contribute to a variety of experiences for users.</p> <p>Equitable access to these experiences is guided in section 4. Guide and Typical Plans (Disability Access and Inclusion).</p> <p>Each civic space and park provides a range of experiences and uses depending on its function. Specifically the Key Civic Space in Key Site A, and the Recreational Park typology provide for large scale events and recreation. For guidance refer to section 4. Guide and Typical Plans.</p> <p>Where relevant public art is guided in the individual civic spaces and parks in section 4. Guide and Typical Plans.</p>
<p>2.5.4 Public realm</p> <p>Development delivers an attractive, high quality, accessible, well-connected, multi-functional public realm that:</p> <ol style="list-style-type: none"> creates a safe, comfortable and inviting environment by promoting day and night activity and passive surveillance 	<p>Smart technology plays a specific role in creating safe and inviting environments in the Mill Civic Spaces and Parks.</p> <p>Day activity is promoted in access, furniture, event elements, public amenities, play, active recreation, public art, memorials, commercial buildings, food trucks and wayfinding.</p> <p>Night activity is promoted through access, lighting, event elements, commercial buildings, food trucks, and interim uses.</p> <p>Each civic space and park contributes to safety and comfort, and is inviting in different ways that are appropriate to the function of each typology. Refer to each civic space and park typology in section 4. Guide and Typical Plans for specific guidance.</p>	<p>Place 1 - Mill Central Criteria</p> <p>2.6.1 Land Use Activities</p> <ol style="list-style-type: none"> Development for Interim uses in Mill Central will delivery land uses that promote activity and deliver amenity in the place ... Development in the university and mixed-use area will on key site A, deliver .. a key civic space ... 	<p>Civic Space and Park guidance</p> <p>Interim uses in civic spaces and parks are critical for their functions as vibrant people places. Interim uses and food truck spaces have been guided in individual typologies, where relevant to their functions in section 4. Guide and Typical Plans.</p> <p>Key Civic Spaces are specifically guided in section 4.2 Key Civic Space.</p>
		<p>Place 5 - Mill Green Criteria</p> <p>2.6.5 Built Form and Recreational Opportunities</p> <ol style="list-style-type: none"> Development in Mill Green will provide opportunities for passive and active recreation uses with associated ancillary facilities and amenities in the regional recreation park 	<p>Civic Space and Park guidance</p> <p>Recreation Park attributes are specifically guided in section 4.4 Recreation Park.</p>

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3. TYPES AND FUNCTIONS

The civic spaces and parks in *Mill Central* and *Mill Green* are people-oriented spaces predominantly for socialisation, recreation, active living, entertainment, and appreciation of the natural environment. They are critical to the economic, environmental and social vitality and sustainability of the PDA.

The Mill's vision is to introduce a new network of civic spaces and parks, and improve connection between the district centre (Petrie) and new uses east of the rail line. This network of new and existing civic spaces and parks will provide a variety of experiences, from the urbane, busy places, to calm, green parklands, to active sportsfields. Each type of park needs to achieve its particular role to provide high quality experiences for the community and visitors.

Table 2: Civic Space and Park Types and Functions

The Mill Typology	Primary Function	Role
Key Civic Space	District or Regional Civic Park	Key Civic Spaces are public spaces that provide socialisation, recreation, activities, and entertainment to residents, visitors and workers, in an urban setting.
Public Plaza	Public Plaza	Public Plazas are civic spaces, open to the public, within development lots, which provide socialisation and breakout spaces for employees and visitors. They facilitate pedestrian movement, particularly to major building entries.
Local Park	Local Recreation Park	Local Parks are compact public spaces that provide socialisation and recreation opportunities to the local population in a landscape setting.
Recreation Park	District or Regional Recreation Park	Recreation Parks are public spaces that provide socialisation, recreation, activities, entertainment and a connection to nature, to residents and visitors, in a landscape setting.
Sport Park	District or Regional Sport Park	Sport Parks are public spaces that provide active recreation, with a focus on formal competitive sport activities, to residents and visitors, in a landscape setting.

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4. GUIDE AND TYPICAL PLANS

This guide applies to all civic space and park typologies within *Mill Central* and *Mill Green*. The typical plans and artist's impressions illustrate a desired design expectation.

The following guides civic space and park characteristics and elements such as:

- park area;
- flooding;
- access;
- furniture;
- lighting;
- event elements;
- hardscape;
- softscape;
- pathway network;
- public amenities;
- play;
- active recreation;
- public art;
- memorials;
- car parking;
- commercial buildings;
- food trucks;
- wayfinding; and
- water sensitive urban design.

This guide, and its illustrations, do not prescribe exact locations of civic space and park elements. Elements should be located to maximise user engagement, facilitate pedestrian connectivity, contribute to user comfort, adapt to potential future uses and staging, connect people to living systems, protect conservation areas, and maximise long-term asset value.

4.1 TYPOLOGIES DESIGN GUIDE

The following design guidelines apply to all Park types within the *Mill Central Place Provision*. Refer to Typical Plans for information unique to each typology.

Signage

- a. All Wayfinding Signage as per Moreton Bay Regional Council Region Wayfinding Style Guide (August 2017).

Lighting

- b. Lighting through modular smart lighting integrated with other smart technology.

Play

- c. Playgrounds are naturally shaded by trees and associated vegetation. While trees are establishing built shade solutions are used temporarily in the short to medium term until natural shade becomes effective.
- d. Incorporate accessible and inclusive elements into playgrounds to the extent practicable. Accessible play equipment incorporates evidence-based accessibility features that reflect the following principles:
 - » Not be separated from other forms of play equipment;
 - » Be accessible and appealing to children without a disability as well as those with a disability; and
 - » Encourage interaction between children with different levels of ability.

Disability Access and Inclusion

- e. When planning routes for footpaths, steep gradients are minimised to the extent practicable.
- f. Driveways are designed to meet the road surface at level.
- g. Provide accessible car parking bays at on-street and off-street locations. Accessible parking bays are positioned to enable ease of access to council buildings and facilities, parks and open space.
- h. Ensure that people who use a wheelchair or other mobility device can access park benches/tables in council parks.
- i. Ensure that people of all abilities can access council's public exercise stations.

Water Sensitive Urban Design

- j. Water Sensitive Urban Design elements are to be incorporated as per the Healthy Land and Water¹ *Water Sensitive Urban Design Technical Design Guidelines* and Healthy Land and Water *Water Sensitive Designs* document, as amended or replaced from time to time, and Moreton Bay Regional Council *Planning Scheme Policy - Integrated design*.

¹ The Development Scheme footnote 19 in 2.5.5 Environment references Healthy Waterways. Healthy Waterways is now Healthy Land and Water.

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KEY CIVIC SPACE

Artist's Impression 3: Key Civic Space - urban recreation elements of table tennis and chess board

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4.2 KEY CIVIC SPACE

The following design guidance applies to all new and existing Key Civic Spaces within *Mill Central*. It outlines additional unique guidance for the new Civic Space in Key Site A, as located in *Development Scheme, Framework Plan 1 - Mill Central (2.6.1 Place Provisions - Mill Central)*.

Key Civic Spaces are hubs for vibrant, urban, and civic life. They are inviting destinations for all people, and are spaces that encourage creativity, interaction, collaboration, and foster a sense of community belonging. They can be spaces that encourage innovative thinking, as a place for testing ideas and networking. Key Civic Spaces are highly programmed to ensure their vibrancy in their role as the civic heart. Key Civic Spaces can also provide dedicated areas to respectfully commemorate the past and memorialise significant events and people.



Figure 1: Key Civic Space Typical Plan (for illustration only)

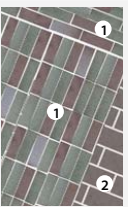
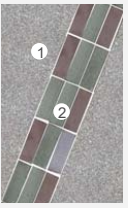
Key Civic Space Attributes

Table 3: Key Civic Space Attributes

Area	Minimum 1,000m ²
Flooding	All above 1% AEP (1 in 100 year ARI) event
Minimum width at any point	30m
Access	<ul style="list-style-type: none"> Minimum 1 access point for maintenance and emergency access 24 hour access to all of the park is maintained for pedestrians
General Furniture	<ul style="list-style-type: none"> <i>Higher use areas</i> use off-the-shelf furniture with bespoke elements <i>Lower use areas</i> use off-the-shelf furniture
Seats and Picnic Tables	<ul style="list-style-type: none"> Seats located along major pathways, pathway junctions/nodes, view/vantage points, and destinations A variety of public seating options for individuals and small groups <p><i>For Key Site A:</i></p> <ul style="list-style-type: none"> A variety of public seating options for individuals, small groups, and large gatherings Moveable seating, stored securely overnight (refer to Shelters and Structures)
Drinking Fountains	<p>Minimum of 1 combined drinking / water bottle refilling fountain</p> <p><i>For Key Site A:</i></p> <ul style="list-style-type: none"> An additional minimum of 1 combined drinking / water bottle refilling fountain
Bins	<ul style="list-style-type: none"> Bins located along major pathways, pathway junctions/nodes, view/vantage points, and destinations <i>In higher use areas:</i> <ul style="list-style-type: none"> bins at 100m intervals along pathways no smart bins required as they will require regular emptying <i>In lower use areas</i> smart bins at 200m intervals to track and alert when it is full, so they can be emptied on demand
Bike Racks	3 bike racks
Shelters and Structures	<ul style="list-style-type: none"> 1 large shelter <i>For Key Site A</i>, in a <i>higher use area</i>, provide secure storage for moveable chairs, other activation elements, and active recreation equipment
Smart Infrastructure	<p><i>In higher use areas:</i></p> <ul style="list-style-type: none"> Free public wifi Banners and provisions for event decorations Data-capture (eg. Pedestrian counters) CCTV Speakers / Community Messaging Event Power Outlets Large Screen/TV or projection screen and projector
Lighting	<ul style="list-style-type: none"> Pedestrian light posts Up-lighting of features <p><i>Additionally for Key Site A:</i></p> <ul style="list-style-type: none"> Artistic projections In ground LED lighting as directional markers

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Event elements	<p>For <i>Key Site A</i> only:</p> <ul style="list-style-type: none"> 1x formal event space of minimum 500m² including a stage, including area for crowd gathering on high quality grass lawn Stage power to include (3 phase where appropriate): <ul style="list-style-type: none"> 1x 10amp 1x 15amp 1x 50amp <p>For all Key Civic Spaces including <i>Key Site A</i>:</p> <ul style="list-style-type: none"> 1x informal event space - a flat open space at least 15x15m that can accommodate a small gathering of people with a surface of either hardscape, grass or combination Power to include 3 phase 32amp every 30 metres 															
Hardscape: Nodes in higher use areas	 <table border="1"> <thead> <tr> <th></th> <th>Material 1</th> <th>Material 2</th> </tr> </thead> <tbody> <tr> <td>Arrangement</td> <td>Angular block patterns</td> <td>Angular block patterns</td> </tr> <tr> <td>Material</td> <td>Bricks (long and thin dimensions) with off white joints</td> <td>In-situ Coloured Concrete and aggregate with sawn joint pattern</td> </tr> <tr> <td>Colours</td> <td>Mix of 40% cool/grey and 60% warm/dark</td> <td> <ul style="list-style-type: none"> Base colour - Warm grey/mauve Aggregate - Dark grey </td> </tr> <tr> <td>Pattern</td> <td>Mix of stretcher bond pattern, and stack bond pattern</td> <td>Stretcher bond</td> </tr> </tbody> </table>		Material 1	Material 2	Arrangement	Angular block patterns	Angular block patterns	Material	Bricks (long and thin dimensions) with off white joints	In-situ Coloured Concrete and aggregate with sawn joint pattern	Colours	Mix of 40% cool/grey and 60% warm/dark	<ul style="list-style-type: none"> Base colour - Warm grey/mauve Aggregate - Dark grey 	Pattern	Mix of stretcher bond pattern, and stack bond pattern	Stretcher bond
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Hardscape: Pathways in lower use areas	Plain grey, brushed concrete															
Softscape	<ul style="list-style-type: none"> Plant selection suitable for an urban environment Shade trees for pedestrian areas No koala attracting trees In <i>higher use areas</i> provide an automated, reticulated irrigation system to establish and maintain the softscape 															
Pathway Network	<ul style="list-style-type: none"> Maintain clear linear spaces for direct pedestrian movement between destinations, laneways, streets and (current or future) building entries Pathways shaded by trees and/or vine arbors In Key Civic Spaces east of the railway line ensure pathway connections to Petrie, Kallangur and new developments as per the <i>Development Scheme</i> 															

Public Amenities	<ul style="list-style-type: none"> A public amenity facility or multiple facilities to include: <ul style="list-style-type: none"> unisex toilets with public hand-washing areas disabled toilets parents facilities (eg. baby change table) <i>Key Site A</i> is to include: <ul style="list-style-type: none"> high needs disability elements (eg. Changing Places toilet or similar) access to sewerage for portable toilets during events
Play	<p>Either:</p> <ul style="list-style-type: none"> 300m² area of sculptural, urban art equipment that can be climbed and/or interacted with; or irregular placement of sculptural, urban art equipment throughout the park, that can be climbed and/or interacted with.
Active Recreation	For <i>Key Site A</i> , include 200m ² of urban recreation elements (eg. table tennis, chess board)
Public Art	<ul style="list-style-type: none"> High quality urban public art Any art in pathways should be non-slip pavement art only <i>Key Site A</i> to include an element that facilitates interchangeable public art work display (eg. clear box with changing/curated art pieces)
Memorials	<p><i>Key Site A</i> to provide:</p> <ul style="list-style-type: none"> Memorial space of maximum 500m² Include Interpretative signage as per <i>Moreton Bay Regional Council Wayfinding Style Guide</i> <p>Memorials are acceptable in other Key Civic Spaces</p>
Car parking	Minimum 2 disabled street parking spaces and minimum one drop off street parking space within 50m of the park. Refer to The Mill Streetscape Guidelines, Section 6. At-Grade Car Park Guide.
Commercial	<i>Key Site A</i> to provide a cafe or restaurant for the purpose of activating the civic space
Food Truck Space	<p><i>Key Site A</i>, in <i>higher use areas</i>, to provide two 8x3m vehicle grade hardstand spaces, which:</p> <ul style="list-style-type: none"> can be co-located together and co-located with event space are publicly accessible by vehicles with no barriers and gates, or provide a place nearby where a key can be picked up (eg. at a Council facility) have no upstand kerb access (eg. either provide a flush kerb or angled kerb) are registered as part of the Moreton Bay Pop Ups Program
Wayfinding	<ul style="list-style-type: none"> Signage as per <i>Moreton Bay Regional Council Wayfinding Style Guide</i> <i>Key Site A</i>, to provide a regional tourism map sign in a prominent, accessible location
Water Sensitive Urban Design (WSUD)	Provide WSUD that focuses on the Healthy Land and Water <i>Water Sensitive Designs</i> document in the categories of "Enhance Landscape Amenity" and "Maximise Benefits to the Community"
Interim Use	<p>Provide a Transitional Activation Strategy to activate the park prior to its completion. Consider opportunities to begin to build a community relationship with the park by engaging local stakeholders in the process. The Strategy should address the following:</p> <ul style="list-style-type: none"> Prior to construction: Activate the park with temporary events, prototypes, installations and engagement activities. During staged construction: Conduct trials to test activation potential of the finalised park, buffer construction with art and activation to key pedestrian connections. Post-construction: Support locally-led activation, program regular activities (cultural, entertainment, artistic performance)

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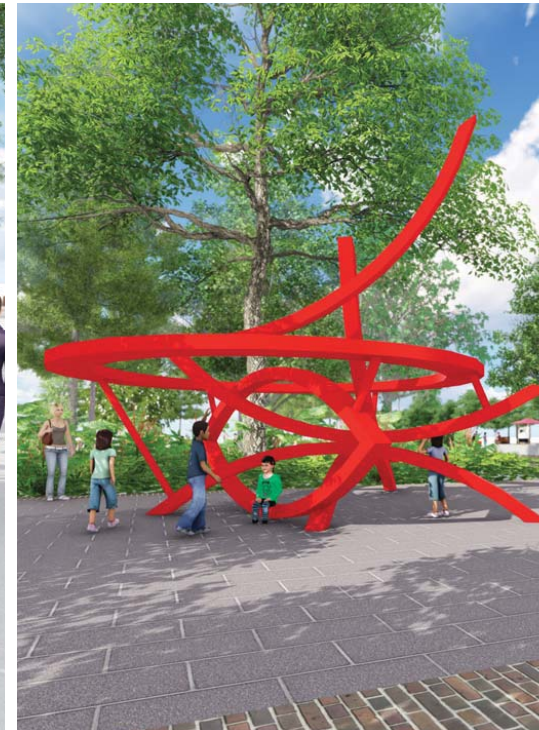
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Artist's Impression 4: Key Civic Space - seating for individuals and small groups with access to free public wifi



Artist's Impression 5: Key Civic Space - restaurant activates the civic space making it more vibrant and safe



Artist's Impression 6: Key Civic Space - high quality public art with interactive elements

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PUBLIC PLAZA

Artist's Impression 7: Public Plaza - provides public connections and places to sit

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4.3 PUBLIC PLAZA

The following design guidance applies to Public Plazas on development land within *Mill Central*.

Public Plazas are places that are adjacent buildings, and are open to the public. They provide an extension of the public realm into private land. The spaces ensure public accessibility and pedestrian connectivity. They are comfortable and engaging for all users.

The Development Scheme PDA-wide criteria 2.6.1 (25) requires development in Mill Central to include a public plaza where:

- a. incorporating a gross leasable area greater than 3,000m²; or
- b. on key site A or B; or
- c. at strategic locations along the district centre main street and neighbourhood hub main street.



Figure 2: Public Plaza Typical Plan (for illustration only)

Public Plaza Attributes

Table 4: Public Plaza Attributes

Area	Minimum area of 5% of the development's gross leasable area, with a minimum area of 200m ² and maximum area of 800m ² (but may be larger), with a maximum length to depth ratio of 1:2 (to achieve for balanced proportions and better usability)
Flooding	All above 1% AEP (1 in 100 year ARI) event
Access	24 hour access to all of the public plaza is maintained for pedestrians
General Furniture	Off-the-shelf furniture with bespoke elements
Seats and Picnic Tables	A variety of public seating options for individuals and small groups
Drinking Fountains, Bins and BBQs	No drinking fountains, bins or BBQs
Bike Racks	Bike racks acceptable
Shelters and Structures	1 shelter area, which can be incorporated into building roof
Smart Infrastructure	Public wifi acceptable, if provided and maintained by land-owner
Lighting	Adequate lighting to provide safe access at night through a combination of: <ul style="list-style-type: none"> ▪ Pedestrian light posts; ▪ Up-lighting of features; and ▪ In ground LED lighting as directional markers
Hardscape	<ul style="list-style-type: none"> ▪ Pavements create a seamless transition between, and complement, the connecting street or laneway typology ▪ Refer The Mill Streetscape Guidelines for street and laneway hardscapes
Softscape	<ul style="list-style-type: none"> ▪ Plant selection suitable for an urban environment ▪ Shade trees for pedestrian areas ▪ No koala attracting trees ▪ It is desirable for the Land-owner to provide automated, reticulated irrigation systems maintained by land-owner
Pathway Network	Maintain clear linear spaces for direct pedestrian movement between destinations, laneways, streets and (current or future) building entries
Public Amenities, Play and Active Recreation	It is desirable to provide similar to elements to the Key Civic Space , if maintained by land-owner
Car parking	No car parks
Food Truck Space	Acceptable to provide one 8x3m vehicle grade hardstand space for a food truck managed and maintained by land owner
Water Sensitive Urban Design (WSUD)	Provide WSUD that focuses on the Healthy Land and Water <i>Water Sensitive Designs</i> document in the categories of "Enhance Landscape Amenity" and "Maximise Benefits to the Community"

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Artist's Impression 8: Public Plaza - comfortable, shady places for public seating



Artist's Impression 9: Public Plaza - informal event and gathering space



Artist's Impression 10: Public Plaza - seamless transition between the adjacent street and Public Plaza

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4.5 LOCAL PARK

The following design guidance applies to Local Parks within *Mill Central* and *Mill Green*. A new Local Park is required east of the railway, in the north.

Local parks are compact recreation spaces with a significant landscape design focus.



Figure 3: Local Park Typical Plan (for illustration only)

Local Park Attributes

Table 5: Local Park Attributes

Area/Flooding	Minimum 5,000m ² with 100% of land above 2% AEP (1 in 50 year ARI event)
Minimum width at any point	20m
Access	Minimum 1 access point for maintenance and emergency access only
General Furniture	Off-the-shelf furniture
Seats and Picnic Tables	Minimum 2 seats or picnic tables
Drinking Fountains	Minimum 1 combined drinking fountain / water bottle refilling station
Bins	Minimum 1 "Smart Bin" to track and alert when it is full, so staff can empty only as needed
Bike Racks	Minimum 2 bike racks
BBQs	No BBQs
Shelters and Structures	Minimum 1 shade shelter
Smart Infrastructure	Free public wifi
Lighting	Pedestrian light posts, where street lighting is not sufficient
Hardscape	Plain grey, brushed concrete
Softscape	<ul style="list-style-type: none"> Native trees in grass with limited groundcovers Include flowering species to add colour and variation
Pathway Network	Connections to and between destinations and existing pathways, shaded with trees
Public Amenities	No public amenities
Play	400m ² playground with softfall
Active Recreation	<ul style="list-style-type: none"> Kick-about space - minimum 20m x 40m grassed space, with a gradient less than 3% Small multi-use activity space which accommodates sports, games and gathering
Car parking	<ul style="list-style-type: none"> Street parking only No car parking on site
Wayfinding	1x Park Sign as per <i>Moreton Bay Regional Council Wayfinding Style Guide</i>

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Artist's Impression 12: Recreation Park - active recreation space and picnic shelters

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4.4 RECREATION PARK

The following design guidance applies to all Recreation Parks within *Mill Central* and *Mill Green*, particularly the Regional Recreation Park in Mill Green.

Recreation parks are inviting destinations for all people that foster a sense of community belonging. They provide a variety of experiences such as quiet, reflective, natural spaces to lively, vibrant, entertaining destinations.

Given their larger size recreation parks have *higher use areas* and *lower use areas*. Higher use areas are where there are focused activities, playgrounds, gathering spaces, commercial spaces, and public amenities, and thus require a higher level of embellishment and quality.

Recreation Parks may interface with Conservation Parks and areas of high environmental significance; where this occurs, Recreation Parks should provide a transitional native vegetated buffer between places for people and places of environmental significance.



Figure 4: Recreation Park Typical Plan (for illustration only)


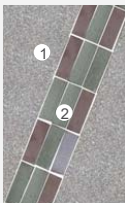
Recreation Park Attributes

Table 6: Recreation Park Attributes

Area	Minimum 4 hectares
Flooding	Internal roads, playgrounds, shelters and other major infrastructure above 2% AEP (1 in 50 year ARI event)
Minimum width at any point	20m
Access	Minimum 2 access points (maintenance and emergency access acceptable)
General Furniture	<ul style="list-style-type: none"> Higher use areas use off-the-shelf furniture with bespoke elements Lower use areas use off-the-shelf furniture
Seats and Picnic Tables	<ul style="list-style-type: none"> Seats located along major pathways, pathway junctions/nodes, view/vantage points, and destinations A variety of public seating options for individuals, small groups, and large gatherings In higher use areas: <ul style="list-style-type: none"> seats at 100m intervals along pathways integrated seating into planter walls provide moveable seating, stored securely overnight (refer to Shelters and Structures) In lower use areas seats at 200m intervals Minimum 2 picnic tables located in grassed passive-recreation spaces under shelters
Drinking Fountains	<ul style="list-style-type: none"> Combined drinking / water bottle refilling fountains located along major pathways Minimum 100m intervals
Bins	<ul style="list-style-type: none"> Bins located along major pathways, pathway junctions/nodes, view/vantage points, and destinations In higher use areas: <ul style="list-style-type: none"> bins at 100m intervals along pathways no smart bins required as they will require regular emptying In lower use areas smart bins at 200m intervals to track and alert when it is full, so they can be emptied on demand
Bike Racks	Minimum 6 bike racks
BBQs	Minimum 3 BBQs
Shelters and Structures	<ul style="list-style-type: none"> Minimum 3 shade shelters (can be incorporated into other structures) In a higher use area, provide secure storage for moveable chairs, other activation elements, and active recreation equipment
Smart Infrastructure	<ul style="list-style-type: none"> In higher use areas: <ul style="list-style-type: none"> Free public wifi Banners and provisions for event decorations Data-capture (eg. Pedestrian counters) CCTV Speakers / Community Messaging EV Charging and Parking Management Event Power Outlets

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Lighting	<ul style="list-style-type: none"> ▪ Pedestrian light posts ▪ Up-lighting of features ▪ In ground LED lighting as directional markers 			
Event elements	<table border="1"> <tr> <td>▪ Informal event space</td> <td>Power at informal event space: <ul style="list-style-type: none"> ▪ 1x 10amp ▪ 1x 15amp ▪ 1x 50amp </td> <td>Power at active recreation: <ul style="list-style-type: none"> ▪ 1x 10amp ▪ 1x 15amp ▪ 1x 50amp </td> </tr> </table>	▪ Informal event space	Power at informal event space: <ul style="list-style-type: none"> ▪ 1x 10amp ▪ 1x 15amp ▪ 1x 50amp 	Power at active recreation: <ul style="list-style-type: none"> ▪ 1x 10amp ▪ 1x 15amp ▪ 1x 50amp
▪ Informal event space	Power at informal event space: <ul style="list-style-type: none"> ▪ 1x 10amp ▪ 1x 15amp ▪ 1x 50amp 	Power at active recreation: <ul style="list-style-type: none"> ▪ 1x 10amp ▪ 1x 15amp ▪ 1x 50amp 		
Hardscape: Nodes in <i>higher use areas</i>		Material 1	Material 2	
	Arrangement	Angular block patterns	Angular block patterns	
	Material	Bricks (long and thin dimensions) with off white joints	In-situ Coloured Concrete and aggregate with sawn joint pattern	
	Colours	Mix of 40% cool/grey and 60% warm/dark	<ul style="list-style-type: none"> ▪ Base colour - Warm grey/mauve ▪ Aggregate - Dark grey 	
	Pattern	Mix of stretcher bond pattern, and stack bond pattern	Stretcher bond	
Hardscape: Pathways in <i>higher use areas</i>		Material 1	Material 2	
	Arrangement	Base pavement	Feature angular banding	
	Material	Coloured concrete with exposed aggregate (large rectangular size)	Bricks (long and thin dimensions) with off white joints	
	Colours	<ul style="list-style-type: none"> ▪ Base colour - Mid-grey ▪ Aggregate - Mix of warm, cool and white 	Mix of 40% cool/grey and 60% warm/dark	
	Pattern	N/A	Stack bond pattern	
Hardscape: Pathways in <i>lower use areas</i>	Plain grey, brushed concrete			
Softscape	<ul style="list-style-type: none"> ▪ Native trees and groundcovers ▪ Include flowering species to add colour and variation ▪ In <i>higher use areas</i> provide an automated, reticulated irrigation system to establish and maintain the softscape 			
Pathway Network	<ul style="list-style-type: none"> ▪ Connections to existing pathways, destinations, and streets ▪ <i>Stacked loop</i> pathway network - accessible loop connecting key destinations; other pathway loops the greater parkland for active recreation ▪ Pathways shaded by trees and/or vine arbors 			

Public Amenities	<ul style="list-style-type: none"> ▪ A public amenity facility or multiple facilities to include: <ul style="list-style-type: none"> » unisex toilets with public handwashing areas » disabled toilets with high needs disability elements (eg. Changing Places toilet or similar) » parents facilities (eg. baby change table) » access to sewerage for portable toilets during events
Play	<ul style="list-style-type: none"> ▪ 2,500m² playground with suitable softfall with a variety of play experiences suited to a range of ages and abilities, to include: <ul style="list-style-type: none"> » At least partially rubber softfall which can be wheeled and walked across; » Play equipment that can be reached from a seated position; » Specific play equipment for people who use a wheelchair (eg. swings, merry go rounds, see saws); » Specific play equipment that provides additional support (eg. swings with back support and harness); and » play equipment that encourages sensory play (eg. sounds, textures)
Active Recreation	<ul style="list-style-type: none"> ▪ Kick-about space - minimum 40 x 40m grassed space, with a gradient less than 3% ▪ Multi-use activity space which accommodates sports, games and gathering ▪ Fitness equipment ▪ Skate Park, BMX track, water play (zero-depth), or other youth adventure recreation facility
Public Art	Public Art incorporated into <i>higher use areas</i>
Memorials	No memorials
Car parking	<ul style="list-style-type: none"> ▪ 40 car spaces (including 2 disability car spaces) ▪ Minimum one bus space ▪ Refer The Mill Streetscape Guidelines, Section 6. At-Grade Car Park Guide
Food Truck Space	In <i>higher use areas</i> , to provide two 8x3m vehicle grade hardstand spaces, which: <ul style="list-style-type: none"> ▪ can be co-located together and co-located with event space ▪ are publicly accessible by vehicles with no barriers and gates, or provide a place nearby where a key can be picked up (eg. at a Council facility) ▪ have no upstand kerb access (eg. either provide a flush kerb or angled kerb) ▪ registered as part of the Moreton Bay Pop Ups Program
Wayfinding	<ul style="list-style-type: none"> ▪ 2x park signs as per <i>Moreton Bay Regional Council Wayfinding Style Guide</i> ▪ At least one Secondary Information Marker as per <i>Moreton Bay Regional Council Wayfinding Style Guide</i>
Interim Use	Provide a Transitional Activation Strategy to activate the park prior to its completion. Consider opportunities to begin to build a community relationship with the park by engaging local stakeholders in the process. The Strategy should address the following: <ul style="list-style-type: none"> ▪ Prior to construction: Activate the park with temporary events, prototypes, installations and engagement activities. ▪ During staged construction: Conduct trials to test activation potential of the finalised park, buffer construction with art and activation to key pedestrian connections. ▪ Post-construction: Support locally-led activation, program regular activities (cultural, entertainment, artistic performance)

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Artist's Impression 13: Recreation Park - shaded pathway connections to destinations



Artist's Impression 14: Recreation Park - intergenerational use of the park, seats and active recreation



Artist's Impression 15: looped path system trails for recreation and fitness

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SPORT PARK

Artist's Impression 16: Sport Park - provide formal, competitive sport activities and informal fitness opportunities

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4.6 SPORT PARK

The following design guidance applies to all Sport Parks within *Mill Central* and *Mill Green*, particularly the Sport and recreation land use in *Mill Central*.

Sport Parks are active spaces for sporting facilities and also provide complementary recreational functions. These parks contribute to the health and wellbeing of communities.

The Sport and recreation land use in *Mill Central* is required to provide sporting facilities that are of a regional standard (such as an aquatic centre, football, rugby, AFL/cricket fields, athletic tracks, basketball/tennis courts and other sports courts).

Sport Parks may interface with Conservation Parks and areas of high environmental significance; where this occurs, Sport Parks should provide a transitional native vegetated buffer between places for people and places of environmental significance.

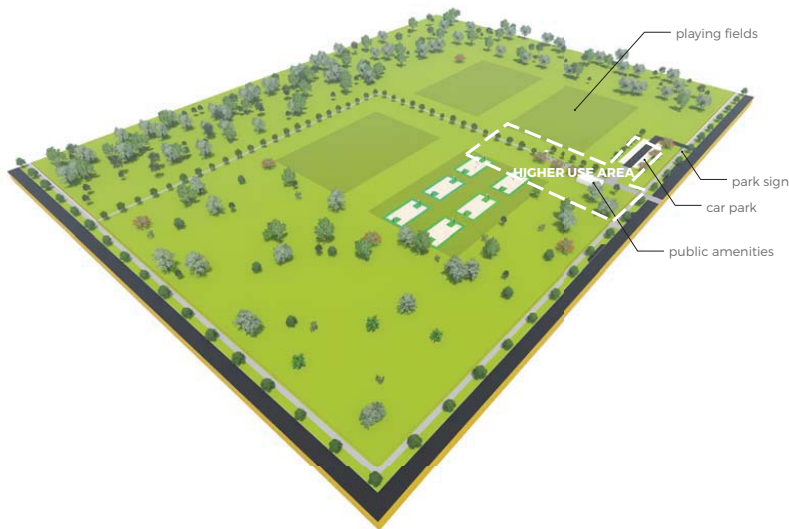


Figure 5: Sport Park Typical Plan (for illustration only)

Sport Park Attributes

Table 7: Sport Park Attributes



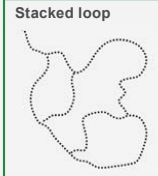
Flooding	<ul style="list-style-type: none"> Buildings and structures above 1% AEP (1 in 100 year ARI) event Formal recreation areas, internal roads, hard courts and vehicle parking above 2% AEP (1 in 50 year ARI) event Playing fields above 5% AEP (1 in 20 year ARI) event
Access	<ul style="list-style-type: none"> All playing fields have adequate vehicle access Minimum 1 access point (maintenance and emergency access acceptable)
General Furniture	Off-the-shelf furniture
Seats and Picnic Tables	<ul style="list-style-type: none"> Seats located within viewing distance of sports fields, along major pathways, pathway junctions/nodes, view/vantage points, and destinations Seats at 200m intervals Minimum 2 picnic tables located in grassed passive-recreation spaces under shelters or shade trees
Drinking Fountains	Minimum 1 combined drinking fountain / water bottle refilling station
Bins	<ul style="list-style-type: none"> Bins located along major pathways, pathway junctions/nodes, view/vantage points, and destinations In <i>higher use areas</i>: <ul style="list-style-type: none"> bins at 100m intervals along pathways no smart bins required as they will require regular emptying In <i>lower use areas</i> smart bins at 200m intervals to track and alert when it is full, so they can be emptied on demand
Bike Racks	Minimum 8 bike racks
BBQs	Minimum 1 BBQ
Shelters and Structures	<ul style="list-style-type: none"> Minimum 3 shade shelters Side-line shelters to each playing field
Smart Infrastructure	In <i>higher use areas</i> : <ul style="list-style-type: none"> Free public wifi Speakers / Community Messaging Event Power Outlets
Lighting	Pedestrian light posts
Hardscape	Plain grey, brushed concrete
Softscape	<ul style="list-style-type: none"> Native trees in grass with groundcovers Include koala attracting trees In <i>higher use areas</i> provide an automated, reticulated irrigation system to establish and maintain the softscape
Pathway Network	<ul style="list-style-type: none"> Connections to existing pathways, destinations, and streets Stacked loop pathway network - accessible loop connecting key destinations; other pathway loops the greater parkland for active recreation Pathways shaded by trees and/or vine arbors
Public Amenities	3 unisex toilets and 1 disability toilet with baby change table
Active Recreation	Playing field/s or other sporting facilities.
Car parking	Car space number appropriate for play fields, and other sporting facilities
Wayfinding	1x park sign as per <i>Moreton Bay Regional Council Wayfinding Style Guide</i>

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5. GLOSSARY

Unless defined below, or in the *Economic Development Act 2012*, or the *Development Scheme*, the *Moreton Bay Regional Council Planning Scheme* definitions apply to this guideline.

Development Scheme	The Mill at Moreton Bay Priority Development Area - Development Scheme (August 2017) - https://www.dsdmip.qld.gov.au/edq/the-mill-at-moreton-bay.html
Higher use areas	Park areas likely to have more pedestrian activity and are located in close proximity to Entry Streets, Main Streets or Shared Streets, and/or at key destinations within parks
Key Site A	As per <i>Development Scheme</i> 2.6.1 Development Criteria - Mill Central
Key Site B	As per <i>Development Scheme</i> 2.6.1 Development Criteria - Mill Central
Lower use areas	Park areas with less pedestrian activity
Mill Central	A Place within the <i>Development Scheme</i> . Refer to Map 2 and section 2.6.1 Place 1 - Mill Central.
Mill Green	A Place within the <i>Development Scheme</i> . Refer to Map 2 and section 2.6.5 Place 5 - Mill Green.
Stacked loop	<p>Trails and pathways are looped so that there is a common start and end point. Looped trails and pathways allow a varied experience for the user that doesn't repeat and reduces the maintenance usually seen on linear trails and pathways. Stacked loops offer a variety of travel experiences.</p> <div style="display: flex; justify-content: space-around; align-items: flex-start;"> <div style="text-align: center;"> <p>Linear</p>  </div> <div style="text-align: center;"> <p>Loop</p>  </div> <div style="text-align: center;"> <p>Stacked loop</p>  </div> </div>

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#2 Streetscape Guidelines



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**The Mill at Moreton Bay
Streetscape Guidelines**

Revision 4.0
Issued 2 August 2019

Report Authors and Contributors

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Contributor



Reviewed by the Mill Design Review Panel

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Artist's Impression 1: Urban Village Street Typology - people and vehicular movement

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1. INTRODUCTION

The Mill Priority Development Area (PDA) aims to be a world-class destination, and as such its design and planning must be exemplary.

The Streetscape Guidelines have been produced to assist in the delivery of The Mill at Moreton Bay Priority Development Area (PDA) vision for an active and embellished public realm, and high quality landscape and urban design outcomes.

These guidelines direct the character and experiential qualities of streetscapes as places for people, movement, active transport, and activity.

They assist the design, planning and assessment of streets and laneways in *Place 1 - Mill Central* and *Place 5 - Mill Green* in the PDA, and provide the designer with a clear indication of the outcomes that are desired.

These guidelines will be utilised by Moreton Bay Regional Council (MBRC), as Minister Economic Development Queensland (MEDQ) delegate, in the assessment of street and laneway proposals in *Mill Central* and *Mill Green*, and the Design Review Panel (DRP) in its role of providing specific design advice.

These guidelines also provide clarity to investors and the community on the streets and laneways expected in *Mill Central* and *Mill Green*.

The standards set out in these guidelines are intended to provide a degree of certainty and clarity of interpretation of the *Development Scheme*, however, as guidelines, they should also be afforded some flexibility in application. Proponents may therefore propose innovative and alternative solutions that demonstrate achievement of the PDA vision, relevant structural elements, PDA-wide criteria and place provisions in the *Development Scheme*.

These guidelines are an evolving document and may be revised, from time to time, to reflect new findings or recommendations made by further planning work for the PDA and on-going review of development outcomes achieved across *Mill Central* and *Mill Green*.

The Mill Streetscape Guidelines are a companion to The Mill Civic Space and Park Guidelines and other future Mill Guidelines.

RELATIONSHIP TO THE DEVELOPMENT SCHEME

The Mill Streetscape Guidelines are 'guidance material' and are to be taken into account in the preparation and assessment of PDA development applications in accordance with sections 2.1.3, 2.2.2 and 2.6 of the *Development Scheme*.

These guidelines are required by the *Development Scheme's* Implementation Strategy to support the achievement of the vision for the Mill. The relevant Implementation Strategy action is:

4.6 Action 2. Develop additional guidance material as needed to assist in communicating the criteria within the development scheme (for example street profiles and cross sections).

As per the Implementation Strategy, these guidelines will assist in communicating the *Development Scheme* criteria.

These guidelines further refine specific criteria as per **2. Development Scheme Criteria** in this guideline (page 2).

RELATIONSHIP TO THE MORETON BAY REGIONAL COUNCIL PLANNING SCHEME

The Moreton Bay Regional Council Planning Scheme is also 'guidance material' and where relevant is taken into account in the preparation and assessment of PDA development applications (refer to section 2.2.12 of the *Development Scheme*).

Where these guidelines contain variations to aspects of the planning scheme, these guidelines prevail.

These guidelines prevail over the following public realm related outcomes, which the Moreton Bay Regional Council Planning Scheme also address:

- street and road cross section attributes; and
- street and road typology cross sections.

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2. DEVELOPMENT SCHEME CRITERIA

These guidelines provide further detail to specific PDA-wide criteria and Place Provision criteria in the *Development Scheme*. The following is a list of the relevant *Development Scheme* criteria and what part of this guideline provides further guidance that would satisfy and deliver what is required.

Table 1: Development Scheme Criteria and its guidance in this document

PDA-wide Criteria	Streetscape guidance
<p>2.5.2 Urban Design</p> <p>Development delivers high quality urban design by ensuring the form, type and arrangement of buildings, structures, streets and public spaces:</p> <ol style="list-style-type: none"> create an appealing, active, inclusive and vibrant urban environment delivers exemplary hard and soft streetscape, utilising sub-tropical design and water sensitive urban design principles, that soften and enhance the physical and visual amenity of the locality. 	<p>The function and roles of each Streetscape typology, as per section 3. Types and Functions, contribute to an appealing, active and vibrant urban environment.</p> <p>Each streetscape typology has a hardscape and softscape approach that creates its identity and fulfils its function.</p> <p>Hardscape, softscape and water sensitive urban design guidance is provided in each Streetscape in section 5. Guide and Cross Sections.</p>
<p>2.5.3 Street and movement network</p> <p>Development delivers a high quality street and movement network as well as related infrastructure for pedestrians, cyclists and vehicles that:</p> <ol style="list-style-type: none"> has a clear hierarchy and is easy to navigate with a well-connected, logical network of routes, intersections and spaces minimises conflict between pedestrians, cyclists and motor vehicles through appropriate design minimises the need for service vehicles to park, stop or queue on the public road network provides for equitable access for all members of the public provides end-of-trip facilities for pedestrians and cyclists 	<p>A variety of streetscape types contribute to a logical network.</p> <p>Each streetscape type provides differing elements to suit its hierarchy in the network. For guidance refer to section 5. Guide and Cross Sections.</p> <p>Each user group has designated spaces in a street to minimise conflict. These spaces are described in section 4. Attributes and illustrated in section 5. Guide and Cross Sections.</p> <p>There is no waste collection on the public roads as per Table 3: Streetscape Cross Section Attributes — Carriageway.</p> <p>Equitable access to these experiences is guided in section 5. Guide and Cross Sections (Disability Access and Inclusion).</p> <p>End of trip facilities such as bike racks and drinking/water-bottle refill stations are provided as per the type of street in section 5. Guide and Cross Sections.</p>

PDA-wide Criteria	Streetscape guidance
<p>2.5.4 Public realm</p> <p>Development delivers an attractive, high quality, accessible, well-connected, multi-functional public realm that:</p> <ol style="list-style-type: none"> incorporate 'smart city' concepts and technology e.g. fibre optic and Wi-Fi networks, smart lighting, connected sensors and CCTV, power and waste systems integration and real time traffic and parking assessment and tracking 	<p>'Smart City' concepts are integral to the streetscape design. Table 3: Streetscape Cross Section Attributes — Carriageway provides more details about what 'Smart Technology' is appropriate to each street type.</p>

Place 1 - Mill Central Criteria	Streetscape guidance
<p>2.6.1 Built Form and Urban Design</p> <p>Development in Mill Central will:</p> <ol style="list-style-type: none"> where a neighbourhood hub main street is designed as a local street, be able to be closed to through traffic for special events and transform to a more active and flexible space. 	<p>Activation of the neighbourhood main street is critical to its function to be a vibrant and active, contemporary street. A suggested action to ensure the ease and affordability to close the street to traffic for special events is outlined in section 5.3 Typology B Main Street (Innovative suggestion).</p>

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Artist's Impression 2: Entry Street Typology - welcoming formal tree-lined boulevard



3. TYPES AND FUNCTIONS

Urban streets which support appropriate vehicle movement are critical to the functioning and amenity of *Mill Central*.

Some are focused principally on vehicle movement and will use landscaping and other design strategies to help manage vehicle behaviour and speeds.

Other streets are designed to integrate pedestrian, cyclist and vehicle movement, with the amenity and safety needs of pedestrians and cyclists being prioritised.

There are eleven street types in the precinct with differing functions and roles, as summarised in Table 2.

While streets are typically in dedicated road reserves, some streets and laneways may be on private land, but still publicly available.

Table 2: Street Types and Functions

Guideline Typology	Primary Function	Role
A - Entry Street	District Collector Street	Entries into Mill Central connecting to Arterial Roads. Creates a sense of arrival.
B - Main Street	Local Collector Street	Neighbourhood hub main street (as identified in the <i>Development Scheme</i>). The District Centre Main Street is a State Road, and will need to be guided by Department Transport and Main Roads.
C - Urban Village Street	Local Collector Street	Provides movement and connections throughout the Mill Priority Development Area.
D - Connector Street	Local Collector Street	Connection between two streets.
E - Railway Interface Street	Local Collector Street	Streets adjacent the railway corridor. Provides a link for commuters and buffer to the development along the rail corridor.
F - Shared Zone Street	Local Collector Street	Streets that prioritise pedestrian crossings between destinations.
G - Park Street	Access Street	Access to or within parks in Mill Central and Mill Green.
H - Mid Block Laneway	Access Street or Driveway	Access between or within development lots. May be on dedicated road reserves or on private land, but publicly accessible.
I - Service Laneway	Access Street or Driveway	Access between or within development lots for the purpose of service. May be on dedicated road reserves or on private land, but publicly accessible.
J - Shared Laneway	Access Street or Driveway	Access between or within development lots - shared between pedestrians, cyclists and vehicles. May be on dedicated road reserves or on private land, but publicly accessible.
K - Pedestrian Laneway	Pathway	Access between or within developments lots by pedestrians and cyclists. May be on dedicated road reserves or on private land, but publicly accessible.

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4. ATTRIBUTES

This section includes attributes and guidance for the carriageway and verges of *Mill Central* (see Figure 1). Carriageway includes:

- reserve widths
- bus
- lighting
- driveway access
- waste
- medians
- traffic lanes
- bike lanes
- on-street parking

Verge includes:

- verge width
- public art
- planting beds
- trees
- street furniture
- on-street dining
- dedicated pathways
- awnings
- interim treatments to lot boundaries

For detailed cross sections and guidance on softscape, hardscape and furniture for each street type refer to section 5. **Guide and Cross Sections**.



Figure 1: Streetscape Typical Cross Section for reference

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4.1 CARRIAGEWAY ATTRIBUTES

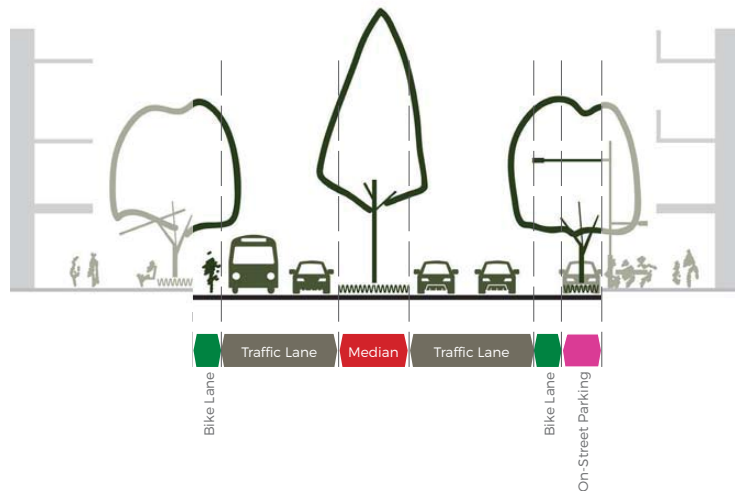


Figure 2: Streetscape Typical Cross Section for reference— CARRIAGEWAY

Table 3: Streetscape Cross Section Attributes – Carriageway

Street Typology	A Entry	B Main Street	C Urban Village
Total Reserve Width	31.7m	27.8m	25.7m
Intended Speed Environment	40 km/h	40 km/h	40 km/h
Bus Route	Yes	Yes	Yes
Cycle	Dedicated bike lanes. 1.5m	Dedicated bike lanes. 1.5m	On road bicycle awareness zone with signs to indicate when entering zone, and painted bicycle markers.
Smart Lighting	Vehicular (V5) & Pedestrian (P3)	Vehicular (V5) & Pedestrian (P3)	Vehicular (V5) & Pedestrian (P3)
Smart Pole components	<ul style="list-style-type: none"> Banners Data-capture (eg. Pedestrian counters) CCTV 	<ul style="list-style-type: none"> Wifi Banners Data-capture (eg. Pedestrian counters) CCTV Speakers / Community Messaging EV Charging and Parking Management Event Power Outlets 	<ul style="list-style-type: none"> EV Charging and Parking Management
Waste Collection	No	No	No
Median	1.5m minimum planted	No median	Only as required for traffic and pedestrian crossing purposes. 1.5m minimum for planting.
Traffic Lane Width	3.3m	3.3m	4.25m
Bike Lane width	1.5m	1.5m	No
On-Street Parking	No on-street parking	<ul style="list-style-type: none"> Indented parallel parking. 2.6m wide Disability car parks as per other standards 	<ul style="list-style-type: none"> Indented parallel parking. 2.6m wide Disability car parks as per other standards

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D Connector	E Railway Interface	F Shared Zone	G Park	H Mid-Block Laneway	I Service Laneway	J Shared Laneway	K Pedestrian Laneway
23.7m	23.7m	17.4m	18.2m	16m	No reserve	No reserve	No reserve
40 km/h	40 km/h	10 km/h	10 km/h	30km/h	30km/h	20km/h	N/A
Yes	Yes	Yes	No	No	No	Yes	No
On road bicycle awareness zone with signs to indicate when entering zone, and painted bicycle markers.	On road bicycle awareness zone with signs to indicate when entering zone, and painted bicycle markers.	On road bicycle awareness zone with signs to indicate when entering zone, and painted bicycle markers.	None	Shared on road. No extra road width to be provided.	Shared on road. No extra road width to be provided.	Shared on road. No extra road width to be provided.	Shared on road. No extra road width to be provided.
Vehicular (V5) & Pedestrian (P3)	Vehicular (V5) & Pedestrian (P3)	Vehicular (V5) & Pedestrian (P3)	As required	Vehicular (V5) & Pedestrian (P3)	Vehicular (V5) & Pedestrian (P3)	Vehicular (V5) & Pedestrian (P3)	Pedestrian (P3)
<ul style="list-style-type: none"> Data-capture (eg. Pedestrian counters) CCTV EV Charging and Parking Management 	<ul style="list-style-type: none"> Data-capture (eg. Pedestrian counters) CCTV 	<ul style="list-style-type: none"> Wifi Banners Data-capture (eg. Pedestrian counters) CCTV Speakers / Community Messaging Event Power Outlets 	<ul style="list-style-type: none"> Data-capture (eg. Pedestrian counters) CCTV Event Power Outlets 	<ul style="list-style-type: none"> Data-capture (eg. Pedestrian counters) CCTV EV charging and parking management 	<ul style="list-style-type: none"> Data-capture (eg. Pedestrian counters) CCTV EV charging and parking management 	<ul style="list-style-type: none"> Wifi Banners Data-capture CCTV Speakers / Community messaging Event power outlets 	<ul style="list-style-type: none"> Wifi Banners Data-capture CCTV
No	No	No	Yes	No	Yes	Yes	No
Only as required for traffic and pedestrian crossing purposes. 1.5m minimum for planting.	Only as required for traffic and pedestrian crossing purposes. 1.5m minimum for planting.	Only as required for traffic and pedestrian crossing purposes. 1.5m minimum for planting.	Only as required for traffic and pedestrian crossing purposes. 1.5m minimum for planting.	No	No	No	N/A
4.25m	4.25m	4.5m	3.3m	4.2m maximum	3.5m maximum	3.5m maximum	No
No	No	No	No	No	No	No	No
<ul style="list-style-type: none"> Indented parallel parking. 2.6m wide Disability car parks as per other standards 	<ul style="list-style-type: none"> Indented parallel parking. 2.6m wide Disability car parks as per other standards 	Indented parallel parking at 2.6m wide for "Kiss and Ride" or Commercial lay-bys only	All street parking types acceptable	Acceptable	N/A	N/A	N/A

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4.2 VERGE ATTRIBUTES

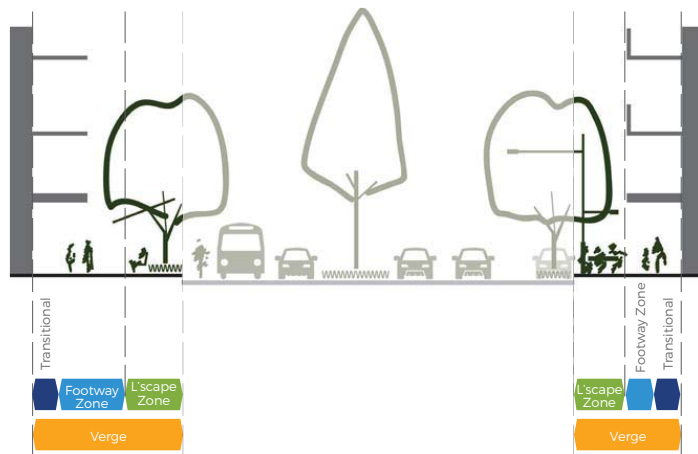


Figure 3: Streetscape Typical Cross Section for reference—Verge

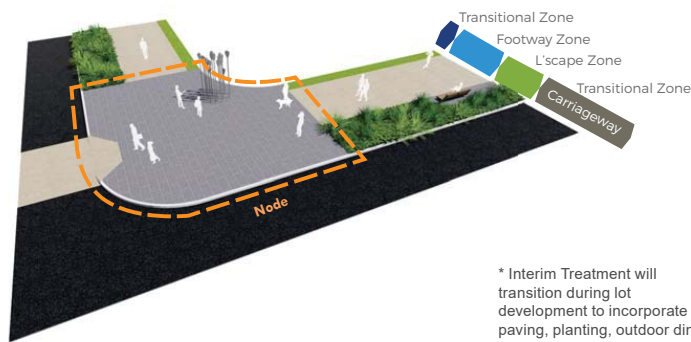


Figure 4: Streetscape Typical Node

Table 4: Streetscape Cross Section Verge Attributes – Verge

Street Typology		A Entry	B Main Street	C Urban Village
Verge		Typically 6.5m	Typically 6.5m	Typically 6m
Public Art		<ul style="list-style-type: none"> Required Any form of public art is acceptable in Landscape Zone Transitional Zones, and Nodes Only non-slip pavement art in Footway Zone. 	<ul style="list-style-type: none"> Required Any form of public art is acceptable in Landscape Zone Transitional Zones, and Nodes Only non-slip pavement art in Footway Zone. 	Acceptable, but only non-slip pavement art in Footway Zone
Landscape (L'Scape Zone)	Planting Bed Width	<ul style="list-style-type: none"> No turf Minimum 2m 	<ul style="list-style-type: none"> No turf Minimum 2m 	<ul style="list-style-type: none"> Planting and/or turf Minimum 2m
	Trees	Yes	Yes	Yes
	Street Furniture	<ul style="list-style-type: none"> Seats Drinking Fountains Bike Racks Bollards 	<ul style="list-style-type: none"> Seats Tables Water bottle refilling station Drinking fountains Bins Bike Racks Bollards 	<ul style="list-style-type: none"> Seats Smart Bins (to alert when full) Bike Racks Bollards
On-Street Dining		No	Yes (but excluded from dedicated pathway area)	Yes (only within Transitional Zone)
Footway Zone	Dedicated pathway	<ul style="list-style-type: none"> 3m minimum width Hardscape pavement only 	<ul style="list-style-type: none"> 4m minimum width High quality finishes 	<ul style="list-style-type: none"> 3m minimum width Hardscape pavement only
	Awnings	<ul style="list-style-type: none"> Not required, however acceptable No turf or trees under awnings; groundcovers and shrubs planting acceptable 	<ul style="list-style-type: none"> Yes, required No turf or trees under awnings; groundcovers and shrubs planting acceptable 	<ul style="list-style-type: none"> Not required, however acceptable No turf or trees under awnings; groundcovers and shrubs planting acceptable
Transitional Zone	Interim treatment to Lot Boundary*	Minimum 1m turf, planting or decomposed granite	Minimum 1m turf, planting or decomposed granite	Minimum 1m turf, planting or decomposed granite

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D Connector	E Railway Interface	F Shared Zone	G Park	H Mid-Block Laneway	I Service Laneway	J Shared Laneway	K Pedestrian Laneway
Typically 5m	Typically 5m	Typically 4.2m	No minimums or maximums	Typically 5m	Typically 4m	Typically 6m	Full width (typically 9m)
Acceptable, but only non-slip pavement art in Footway Zone	<ul style="list-style-type: none"> Required Any form of public art is acceptable in Landscape Zone Transitional Zones, and Nodes Only non-slip pavement art in Footway Zone. 	<ul style="list-style-type: none"> Required Any form of public art is acceptable in Landscape Zone Transitional Zones, and Nodes Only non-slip pavement art in Footway Zone. 	Required to incorporate art elements into park entry signage/statement	Acceptable, but only non-slip pavement art in Footway Zone	Acceptable, but only non-slip pavement art in Footway Zone	Acceptable, but only non-slip pavement art in Footway Zone	Acceptable, but only non-slip pavement art in Footway Zone
<ul style="list-style-type: none"> No turf Minimum 2m 	<ul style="list-style-type: none"> No turf Minimum 2m 	<ul style="list-style-type: none"> No planting beds or turf 	<ul style="list-style-type: none"> As required 	<ul style="list-style-type: none"> Planting and/or turf Minimum 2m 	<ul style="list-style-type: none"> Planting and/or turf Minimum 2m 	<ul style="list-style-type: none"> Planting and/or turf Minimum 2m 	<ul style="list-style-type: none"> Planting and/or turf Minimum 2m
Yes	Yes	Yes	Yes	Yes	Acceptable	Yes	Yes
<ul style="list-style-type: none"> Seats Drinking fountains Bins Bike Racks Bollards 	<ul style="list-style-type: none"> Seats Water bottle refilling station Bins Bike Racks Bollards 	<ul style="list-style-type: none"> Seats Water bottle refilling station Drinking fountains Bins Bike Racks Bollards 	<ul style="list-style-type: none"> Seats Bollards 	<ul style="list-style-type: none"> Seats/Tables Bike Racks Bollards 	<ul style="list-style-type: none"> Seats/Tables Bike Racks Bollards 	<ul style="list-style-type: none"> Seats/Tables Bike Racks Bollards 	<ul style="list-style-type: none"> Seats/Tables Bike Racks
Yes (but excluded from dedicated pathway area)	No	Yes (but excluded from dedicated pathway area)	Yes	Yes	No	Yes	Yes
<ul style="list-style-type: none"> 3m minimum width Hardscape pavement only 	<ul style="list-style-type: none"> 2.5m minimum width Hardscape pavement only 	<ul style="list-style-type: none"> 3m minimum width Hardscape pavement only 	As required in park design	3m minimum width	2m minimum width	3m minimum width	3m minimum width
<ul style="list-style-type: none"> Not required, however acceptable No turf or trees under awnings; groundcovers and shrubs planting acceptable 	<ul style="list-style-type: none"> Not required, however acceptable No turf or trees under awnings; groundcovers and shrubs planting acceptable 	<ul style="list-style-type: none"> Not required, however acceptable No turf or trees under awnings; groundcovers and shrubs planting acceptable 	N/A	<ul style="list-style-type: none"> Not required, however acceptable No turf or trees under awnings; groundcovers and shrubs planting acceptable 	<ul style="list-style-type: none"> Not required, however acceptable No turf or trees under awnings; groundcovers and shrubs planting acceptable 	<ul style="list-style-type: none"> Not required, however acceptable No turf or trees under awnings; groundcovers and shrubs planting acceptable 	<ul style="list-style-type: none"> Not required, however acceptable No turf or trees under awnings; groundcovers and shrubs planting acceptable
Minimum 1m turf, planting or decomposed granite	Minimum 1m turf, planting or decomposed granite	Minimum 1m turf, planting or decomposed granite	Minimum 1m turf, planting or decomposed granite	N/A	N/A	N/A	N/A

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Artist's Impression 3: Main Street Typology - highly pedestrian focused, active frontages with awnings and formal shade trees in grates

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5. GUIDE AND CROSS SECTIONS

The following guide applies to all street types, and provides guidance about softscape, signage, lighting, innovation, and disability access and inclusion.

The cross sections illustrate attributes from section 4. **Attributes**, and provide further guidance for each street typology in regards to softscape, hardscape, and furniture.

5.1 GENERAL DESIGN GUIDE

The following general design guidelines apply to all Streetscapes within the *Mill Central* and *Mill Green* Place Provision. Refer to Cross Sections for information unique to each typology.

Softscape

- a. Select tree and other plant species that are culturally significant to the site, the suburb of Petrie and North Pine area, or native. Do not use invasive or weed species.
- b. Select:
 - » *landmark* tree species for the purposes of wayfinding and grand entry statements
 - » shade tree species that provide shade through a large canopy and a clear trunk
- c. Select species that require less watering post-establishment, or that can be passively irrigated.
- d. Select species that are manageable and reasonable to maintain to a level expected from an Urban Centre.
- e. Select no koala attracting species for street trees unless approved by Moreton Bay Regional Council Environmental Services.
- f. Eight cubic metres of uncompacted soil is allowed per street tree using structural soil cells or similar. Where trees are clustered a larger pit or common trenching is used to combine drainage, irrigation provision and to maximise volume. Soil pits extend under road pavement.
- g. Where overland flow is an issue select inorganic mulch (gravel) or an organic mulch type that has an appropriate shape and size which locks together and resists dislodgement such as hoop pine mulch.

Signage

- h. A-frame signage is not placed within designated pathway spaces.
- i. All Wayfinding Signage as per Moreton Bay Regional Council Region Wayfinding Style Guide (August 2017).

Lighting

- j. Street and pedestrian lighting is modular smart lighting poles integrated with other smart technology.
- k. Trees are placed as close to street and pedestrian lights as possible based on Smart Lighting specifications (eg. lighting lumens), tree species and expectations for maintenance.

Innovation

- l. Hubs for future passive hire and storage of small, low speed personal transportation devices eg. bicycle hire and motorised scooters are publicly accessible. Can be located on the verge, in place of an on-street parking spot, located mid-block or co-located with bus stops. There are to be located at a minimum 300m, maximum 500m intervals. These hubs are hardscape areas of at least 10m², with a minimum width of 1.8m.

Disability Access and Inclusion

- m. When planning routes for footpaths, steep gradients are minimised to the extent practicable.
- n. Driveways are designed to meet the road surface at level.
- o. The instances where people may inadvertently restrict the use of footpaths and parking facilities by people with a disability are minimised through design and the installation of furniture items, trees and bollards.
- p. No chicane barriers are installed along shared pathways.
- q. Provide accessible car parking bays on on-street and off-street locations. Accessible parking bays are positioned to enable ease of access to council buildings and facilities, parks and open space.
- r. Bus shelters provided by council include features designed to increase accessibility to public transport. Examples include: a designated wheelchair waiting bay; and directional and warning TGSIs

Water Sensitive Urban Design

- s. Water Sensitive Urban Design elements are to be incorporated as per the *Healthy Land and Water¹ Water Sensitive Urban Design Technical Design Guidelines* and *Healthy Land and Water Water Sensitive Designs* document, as amended or replaced from time to time, and Moreton Bay Regional Council *Planning Scheme Policy - Integrated design*.

¹ The Development Scheme footnote 19 in 2.5.5 Environment references Healthy Waterways. Healthy Waterways are now Healthy Land and Water.

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5.2 TYPOLOGY A | ENTRY STREET

The following design guidance applies to all Entry Streets within *Mill Central*. The Entry Streets are predominately used by vehicles transitioning between or entering/exiting Place Provision boundaries within the PDA. Although these streets will be used by pedestrians, the predominant movements will be vehicular. Entry Streets are dedicated road reserves.

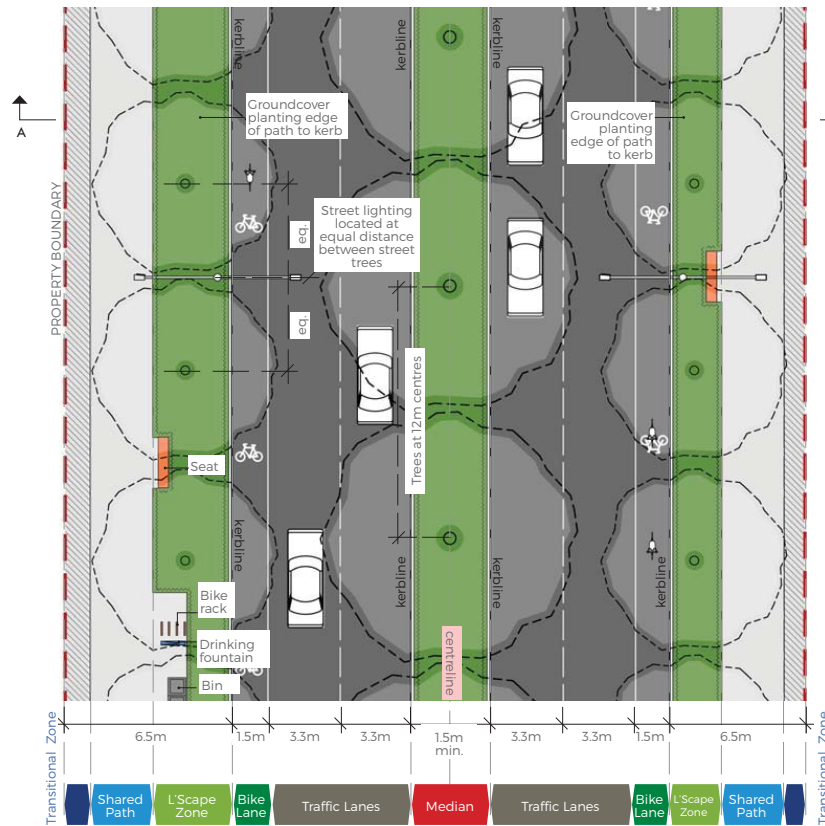


Figure 5: Entry Street Typical Plan

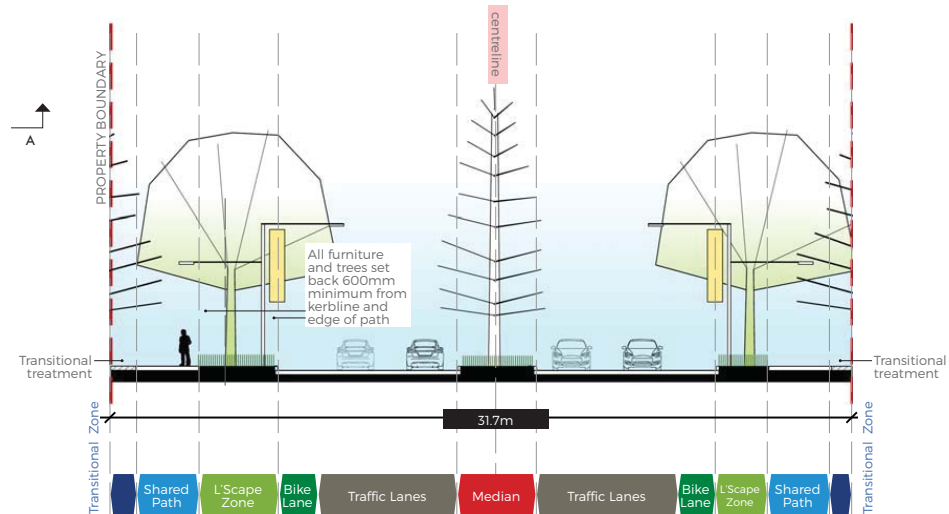


Figure 6: Entry Street Typical Section A

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Entry Street | Softscape

Trees and plantings are formally arranged as a boulevard, with regular spacings, symmetrically mirrored on both sides of the street. Trees are planted in planting beds.

Landmark, tall, wayfinding tree species feature prominently, and are inspired by the Gympie Road, Strathpine species and the historic tree plantings of Anzac Ave.

Trees provide dappled shade for pedestrians and mitigate the heat island effect of the traffic lanes. The trees shade 40% of the landscape and footway zones at full maturity.

An automated, reticulated irrigation system is required to establish and maintain the Entry Street softscape.

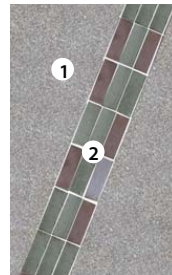
Entry Street | Hardscape

Nodes (Refer Figure 4)



	Material 1	Material 2
Location	Base pavement	Feature banding
Material	Engineered concrete paver (large rectangular size)	Bricks (long and thin dimensions) with off white joints
Colours	Base colour - Warm/grey Aggregate - Mix of warm and white	Mix of 40% cool/grey and 60% warm/dark
Pattern	Stretcher bond	Stack bond pattern

Pathway



	Material 1	Material 2
Location	Coloured concrete with exposed aggregate (large rectangular size)	Feature banding
Material	Base colour - Mid-grey Aggregate - Mix of warm, cool and white	Bricks (long and thin dimensions) with off white joints
Colours	N/A	Mix of 40% cool/grey and 60% warm/dark
Pattern	Stretcher bond	Stack bond pattern

Entry Street | Furniture

Furniture	Locations
Seats	Maximum 200m intervals. Under shade trees.
Drinking/ water-bottle refill fountains	Maximum 200m intervals.
Bike Racks	Maximum 400m intervals.
Bollards	Where there are no other barriers to stop vehicles from entering parks and public plazas (eg. trees and signs) bollards are placed on the boundary of the verge and the park at maximum 1.5m intervals.

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5.3 TYPOLOGY B | MAIN STREET

The following design guidance applies to the Neighbourhood Hub and District Centre Main Street, as defined by the *Development Scheme*. Main Streets are streets with major pedestrian activity and are the hub for commercial activity and activation. Main Streets are dedicated road reserves.

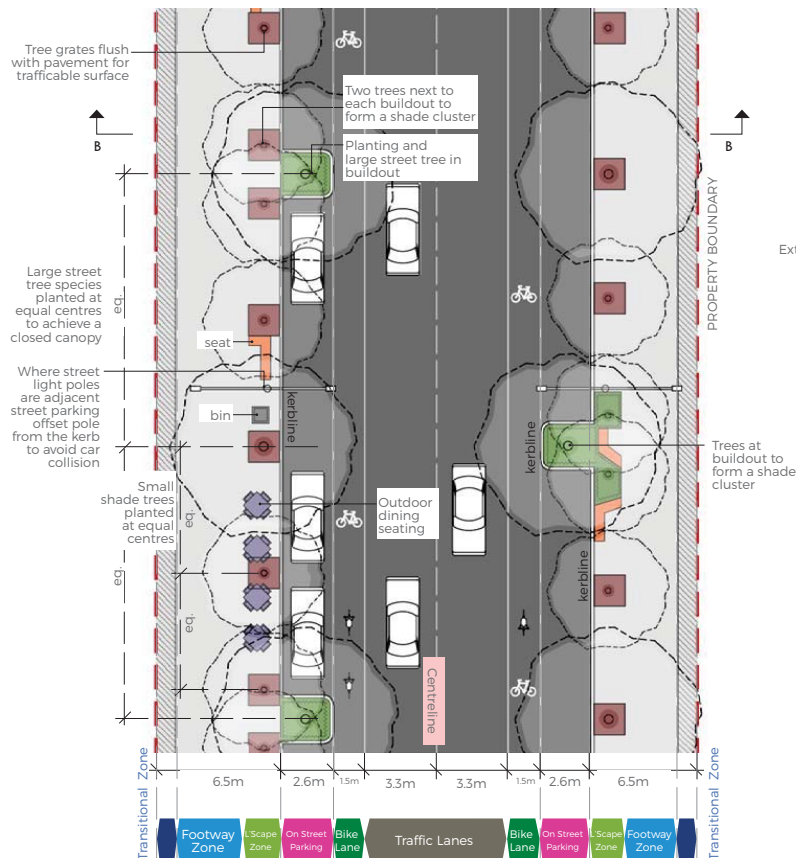


Figure 7: Main Street Typical Plan

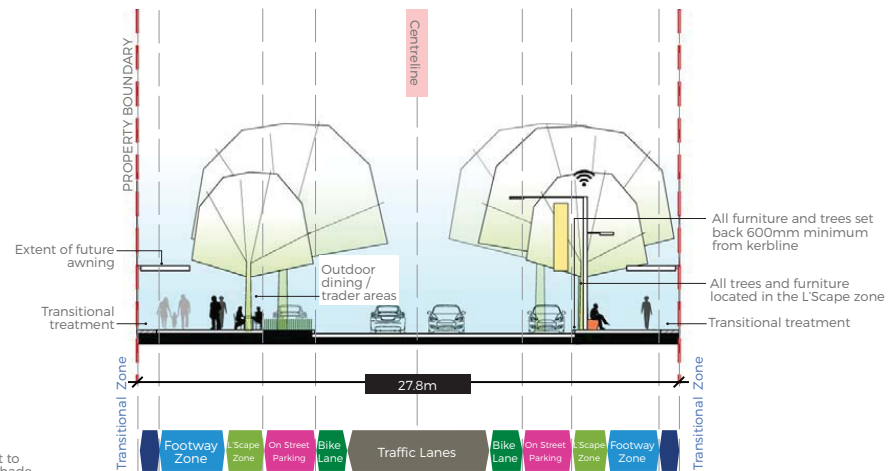


Figure 8: Main Street Typical Section B

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Main Street | Softscape

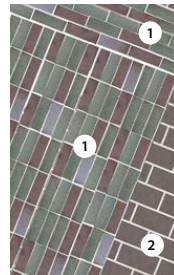
Trees and plantings are formally arranged in regular spacings, or in groves at intersections. Trees are planted in either tree grates or planting beds.

Larger trees species are placed in buildouts to mitigate the heat island effect of the traffic lanes. Tree species in the Landscape Zone are selected to consider awnings. The trees shade 50% of the landscape and footway zones at full maturity.

An automated, reticulated irrigation system is required to establish and maintain the Main Street softscape.

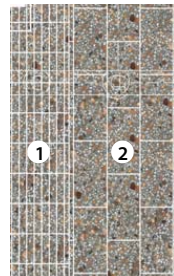
Main Street | Hardscape

Nodes (Refer Figure 4)



	Material 1	Material 2
Location	Nodes	Nodes
Material	Bricks (long and thin dimensions) with off white joints	In-situ Coloured Concrete and aggregate with sawn joint pattern
Colours	Mix of 40% cool/grey and 60% warm/dark	Base colour - Warm grey/mauve Aggregate - Dark grey
Pattern	Mix of stretcher bond pattern, and stack bond pattern	Stretcher bond

Pathway



	Material 1	Material 2
Location	Landscape Zone (areas for street furniture, signage, bus stops, rest spaces)	Footway Zone
Material	Engineered concrete paver (small, long rectangular size)	Engineered concrete paver (large rectangular size)
Colours	Base colour - Mid-grey Aggregate - Mix warm and white	Base colour - Mid-grey Aggregate - Mix warm and white
Pattern	Stack bond	Stretcher bond

Main Street | Furniture

Furniture	Locations
Seats	Maximum 50m intervals. Under shade trees and smart poles.
Drinking/ water-bottle refill fountains	Maximum 100m intervals. At least one along total extent of Main Street, located close to major pedestrian crossing or mid-block.
Bins	Pair of regular waste bin and recycling bin at maximum 100m intervals.
Bike Racks	Maximum 200m intervals.
Bollards	Where there are no other barriers to stop vehicles from entering parks and public plazas (eg. trees and signs) bollards are placed on the boundary of the verge and the park at maximum 1.5m intervals.

Innovative suggestion

Encourage temporary activation by allowing the Neighbourhood Main Street to be easily and affordably closed to vehicle traffic through the following.

- Place the embedded receivers and lids for removable bollards within the road carriageway to assist in road closures for events and markets. Place the removable bollard a maximum of 10m from an intersection to ensure traffic can use an alternate route.
- Removable bollards are stored on verge in Landscape Zone in embedded receivers with lids (locked) when not in use for events and markets.
- This innovation has been added to Albert Street (between Charlotte and Mary Streets) in Brisbane City, and is used for night events outside of peak traffic times.

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5.4 TYPOLOGY C | URBAN VILLAGE STREET

The following design guidance applies to all Urban Village Streets within *Mill Central*. Urban Village Streets accommodate moderate levels of vehicle and pedestrian activity, ultimately acting as a link or collector to the Main Street, Entry Street and other destinations.

Urban Village Streets are dedicated road reserves.

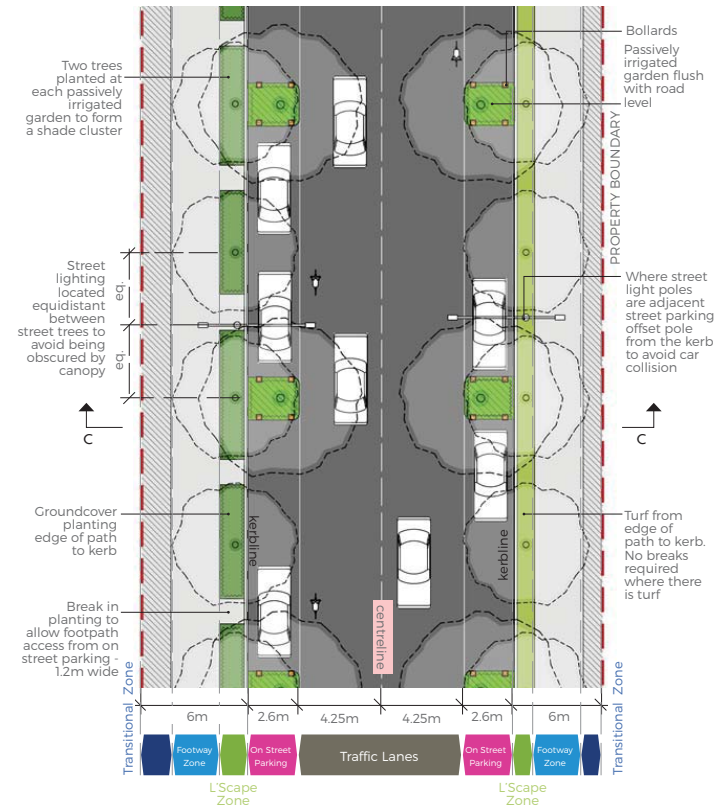


Figure 9: Urban Village Street Typical Plan

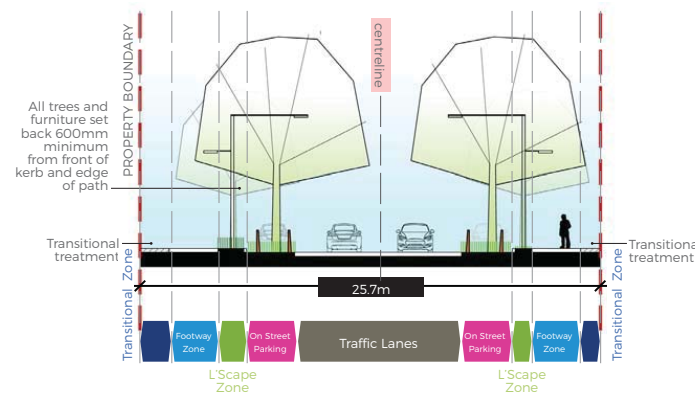


Figure 10: Urban Village Street Typical Section C

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Urban Village | Softscape

Trees and plantings are arranged to create a variation of full and dappled shade for pathway users. Trees are arranged in groves and groupings to alleviate the heat island effect of the traffic lanes. The trees shade 40% of the landscape and footway zones at full maturity.

Trees are planted in planting beds, or if adjacent grass, located in mulch with concrete edges.

An automated, reticulated irrigation system is required to establish and maintain the Urban Village softscape.

Urban Village | Hardscape

Nodes (Refer Figure 4)



	Material 1	Material 2
Location	Base pavement	Feature banding
Material	Engineered concrete paver (large rectangular size)	Bricks (long and thin dimensions) with off white joints
Colours	Base colour - Mid-grey Aggregate - Mix warm and white	Mix of 40% cool/grey and 60% warm/dark
Pattern	Stretcher bond	Stack bond pattern

Footway



	Material 1
Material	Coloured concrete with exposed aggregate (large rectangular size)
Colours	Base colour - Mid-grey Aggregate - Mix of warm, cool and white
Pattern	N/A

Urban Village | Furniture

Furniture	Locations
Seats	Maximum 300m intervals.
Drinking/ water-bottle refill fountains	Maximum 300m intervals.
Bins	Smart waste bin at maximum 300m intervals.
Bike Racks	Minimum 600m intervals.
Bollards	Where there are no other barriers to stop vehicles from entering parks and public plazas (eg. trees and signs) bollards are placed on the boundary of the verge and the park at maximum 1.5m intervals. Bollards to planting bed build outs between car spaces with flush kerbs.

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5.5 TYPOLOGY D | CONNECTOR STREET

The following design guidance applies to all Connector Streets within *Mill Central*. Connector Streets connect the Entry Street or Main Street to other streets. They will act as a transition between these street typologies. Connector Streets are dedicated road reserves.

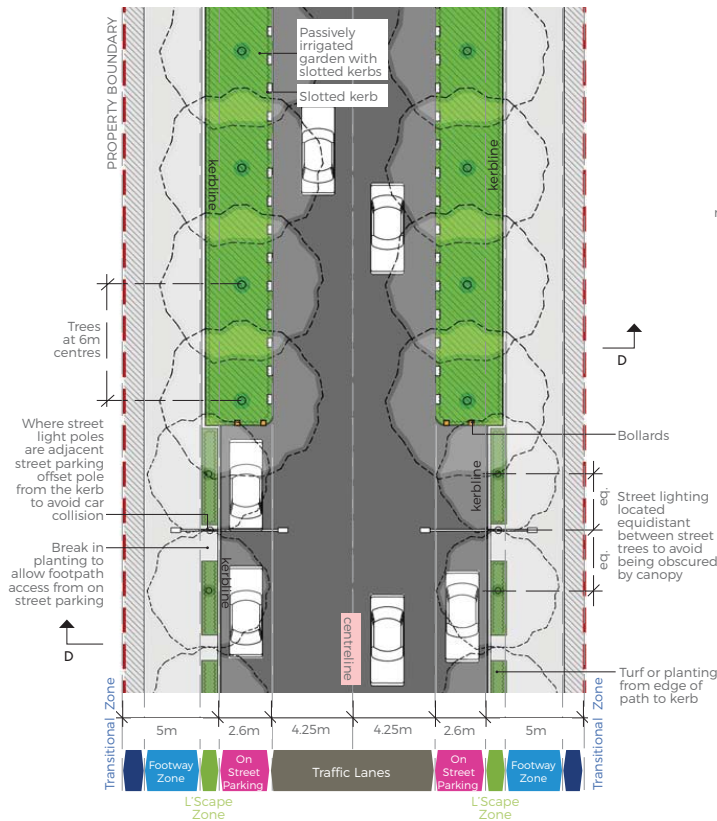


Figure 11: Connector Street Typical Plan

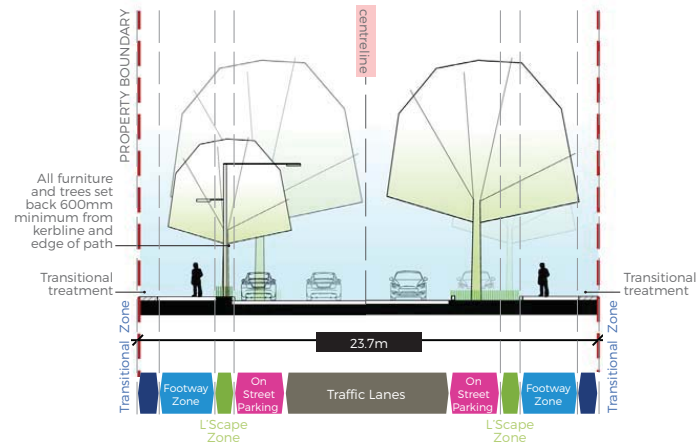


Figure 12: Connector Street Typical Section D

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Connector Street | Softscape

Trees and plantings are formally arranged in regular spacings. The trees shade 40% of the landscape and footway zones at full maturity. Trees are planted in planting beds.

An automated, reticulated irrigation system is required to establish and maintain the Connector Street softscape.

Connector Street | Hardscape

Nodes (Refer Figure 4)



	Material 1	Material 2
Location	Base pavement	Feature banding
Material	Engineered concrete paver (large rectangular size)	Bricks (long and thin dimensions) with off white joints
Colours	Base colour - Mid-grey Aggregate - Mix warm and white	Mix of 40% cool/grey and 60% warm/dark
Pattern	Stretcher bond	Stack bond pattern

Footway



	Material 1
Material	Coloured concrete with exposed aggregate (large rectangular size)
Colours	Base colour - Mid-grey Aggregate - Mix of warm, cool and white
Pattern	N/A

Connector Street | Furniture

Furniture	Locations
Seats	Maximum 50m intervals under shade trees and smart poles.
Drinking/ water-bottle refill fountains	Maximum 200m intervals.
Bins	Pair of regular waste bin and recycling bin at maximum 200m intervals.
Bike Racks	Maximum 200m intervals.
Bollards	Where there are no other barriers to stop vehicles from entering public plazas (eg. trees and signs) bollards are placed on the boundary of the verge and the park at minimum 1.5m intervals. Bollards to planting bed build outs between car spaces with flush kerbs.

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5.6 TYPOLOGY E | RAILWAY INTERFACE STREET

The following design guidance applies to all Railway Interface Streets within *Mill Central*. Railway Interface Streets are located parallel to a railway line. Railway Interface Streets are dedicated road reserves.

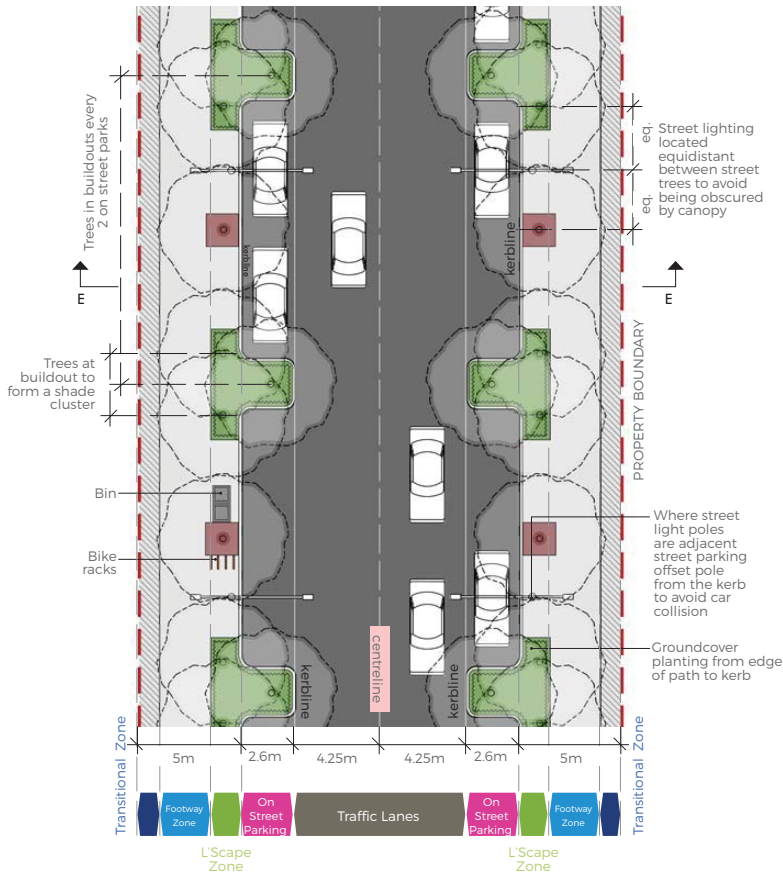


Figure 13: Railway Interface Street Typical Plan

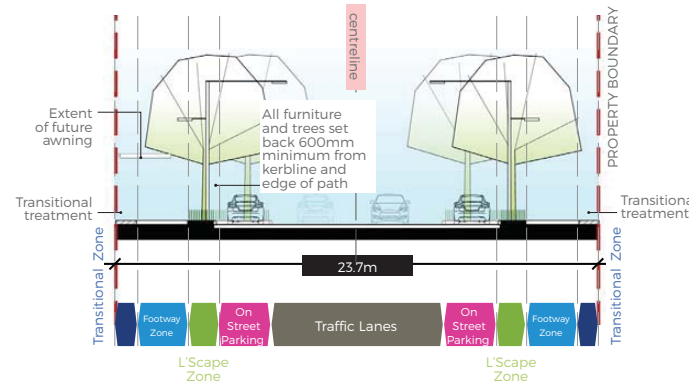


Figure 14: Railway Interface Street Typical Section E

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Railway Interface Street | Softscape

Trees and plantings are formally arranged in regular spacings in groves and larger tree species are placed in buildouts to alleviate the heat island effect of the traffic lanes. The trees will shade 40% of the landscape and footway zones at full maturity. Trees are planted in a combination of tree grates and planting beds.

An automated, reticulated irrigation system is required to establish and maintain the Railway Interface Street softscape.

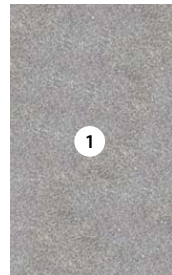
Railway Interface Street | Hardscape

Nodes (Refer Figure 4)



	Material 1	Material 2
Location	Base pavement	Feature banding
Material	Engineered concrete paver (large rectangular size)	Bricks (long and thin dimensions) with off white joints
Colours	Base colour - Mid-grey Aggregate - Mix warm and white	Mix of 40% cool/grey and 60% warm/dark
Pattern	Stretcher bond	Stack bond pattern

Footway



	Material 1
Material	Coloured concrete with exposed aggregate (large rectangular size)
Colours	Base colour - Mid-grey Aggregate - Mix of warm, cool and white
Pattern	N/A

Railway Interface Street | Furniture

Furniture	Locations
Seats	Maximum 200m intervals.
Drinking/ water-bottle refill fountains	Maximum 200m intervals. At least one along total extent of Railway Interface Street, located close to major pedestrian crossing or mid-block.
Bins	Pair of regular waste bin and recycling bin at maximum 200m intervals.
Bike Racks	Maximum 200m intervals.
Bollards	Where there are no other barriers to stop vehicles from entering parks and public plazas (eg. trees and signs) bollards are placed on the boundary of the verge and the park at maximum 1.5m intervals.

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5.7 TYPOLOGY F | SHARED ZONE STREET

The following design guidelines apply to all Shared Zone Streets within *Mill Central*. Shared zone streets experience a high level of pedestrian activity and vehicular activity. They may be dedicated road reserves or located on publicly accessible private land.

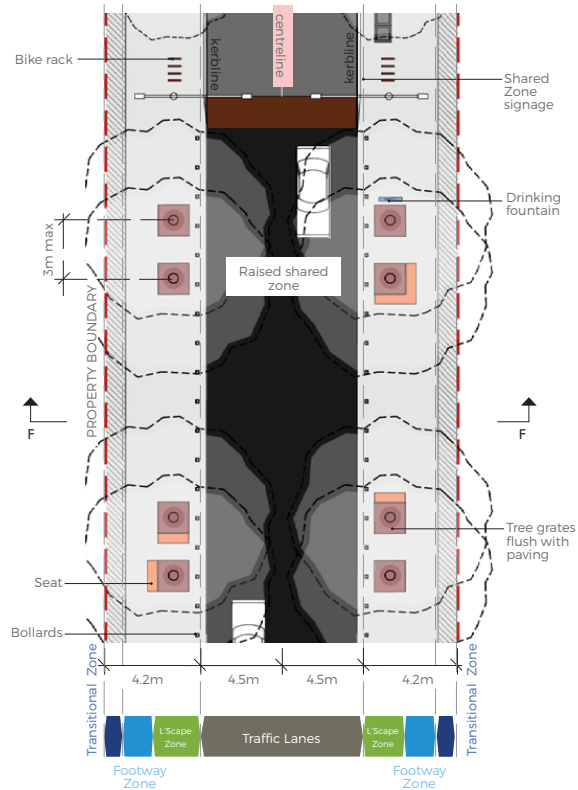


Figure 15: Shared Zone Street Typical Plan

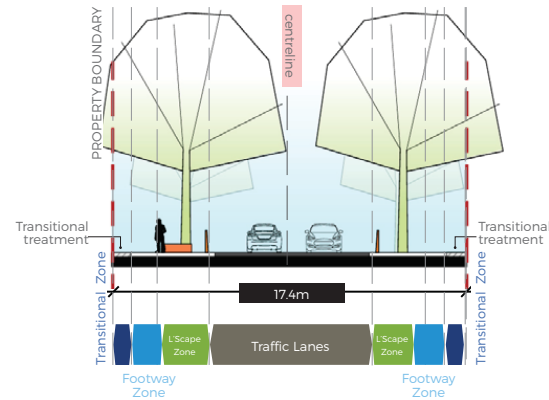


Figure 16: Shared Zone Street Typical Section F

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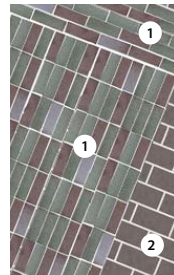
Shared Zone Street | Softscape

Trees are formally arranged in groves, symmetrically mirrored on both sides of the street. Trees are located in tree grates. The trees will shade 30% of the landscape and footway zones at full maturity.

An automated, reticulated irrigation system is required to establish and maintain the Shared Zone Street softscape.

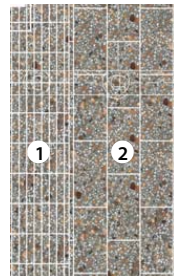
Shared Zone Street | Hardscape

Nodes (Refer Figure 4)



	Material 1	Material 2
Location	Nodes	Nodes
Material	Bricks (long and thin dimensions) with off white joints	In-situ Coloured Concrete and aggregate with sawn joint pattern
Colours	Mix of 40% cool/grey and 60% warm/dark	Base colour - Warm grey/mauve Aggregate - Dark grey
Pattern	Mix of stretcher bond pattern, and stack bond pattern	Stretcher bond

Pathway



	Material 1	Material 2
Location	Landscape Zone (areas for street furniture, signage, bus stops, rest spaces)	Footway Zone
Material	Engineered concrete paver (small, long rectangular size)	Engineered concrete paver (large rectangular size)
Colours	Base colour - Mid-grey Aggregate - Mix warm and white	Base colour - Mid-grey Aggregate - Mix warm and white
Pattern	Stack bond	Stretcher bond

Shared Zone Street | Furniture

Furniture	Locations
Seats or Picnic Tables	Maximum 50m intervals under shade trees and smart poles.
Drinking/ water-bottle refill fountains	Maximum 200m intervals with at least one water bottle refilling station along total extent of Shared Zone Street, located close to major pedestrian crossing or mid-block.
Bins	Pair of regular waste bin and recycling bin at maximum 200m intervals.
Bike Racks	Maximum 200m intervals.
Bollards	Bollards to the extent of the shared zone crossing at maximum 1.5m intervals. Where there are no other barriers to stop vehicles from entering public plazas (eg. trees and signs) bollards are placed on the boundary of the verge and the park at maximum 1.5m intervals.

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5.8 TYPOLOGY G | PARK STREET

The following design guidance applies to park streets within *Mill Central* and *Mill Green*. Park Streets facilitate vehicles accessing parks or car parks, driving through parks, and pedestrians casually crossing. Fauna movement needs to be considered for Park Streets in *Mill Green*.

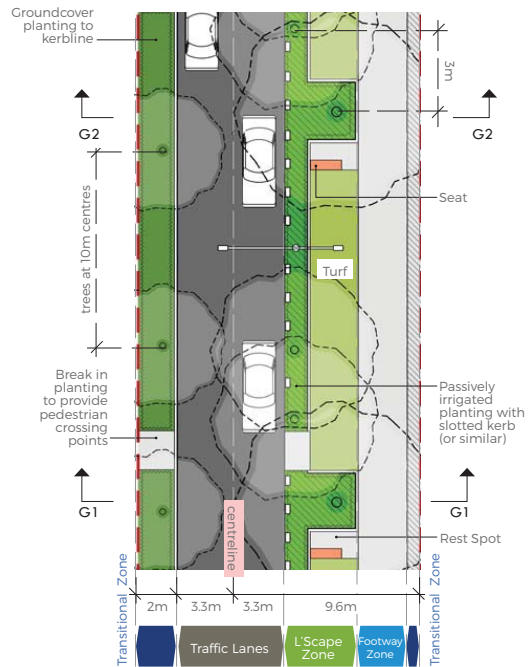


Figure 17: Park Street Typical Plan

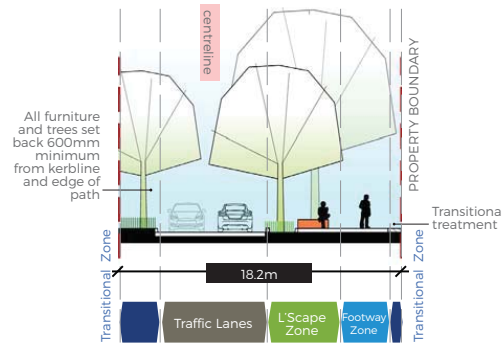


Figure 18: Park Street Typical Section G1

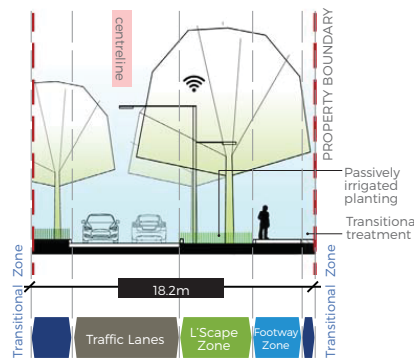


Figure 19: Park Street Typical Section G2

Park Street | Softscape

Trees and planting are to be arranged informally and planted in planting beds, or if adjacent grass, located in mulch with concrete edges. The trees will shade 30% of the landscape and footway zones at full maturity.

Park Street | Hardscape

Plain grey, broomed finished.

Slotted kerb along extent of street.

Park Street | Furniture

Furniture	Locations
Seats	Maximum 200m.
Bollards	Where there are no other barriers to stop vehicles from entering parks (eg. trees and signs) bollards are placed on the boundary of the verge and the park at maximum 1.5m intervals. Bollards to planting beds between car spaces, due to flush kerbs.

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5.9 TYPOLOGY H | MID-BLOCK LANEWAY

The following design guidance applies to Mid-block laneways in *Mill Central*.

Mid-block Laneways facilitate vehicles accessing lots, other laneways, and car parking areas. They may support on-street car parking where required, passively irrigated planting and street trees. Pedestrian movement is limited to the dominant side of the street.

Mid-block laneways may be dedicated road reserves or located on publicly accessible private land.

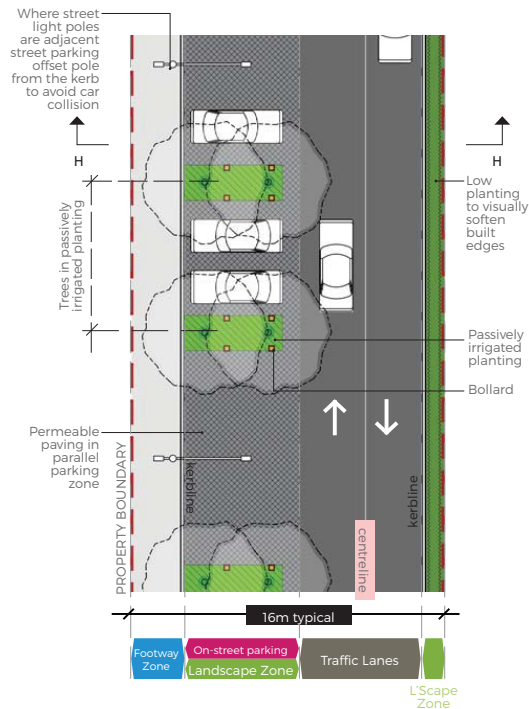


Figure 20: Mid-Block Laneway Typical Plan

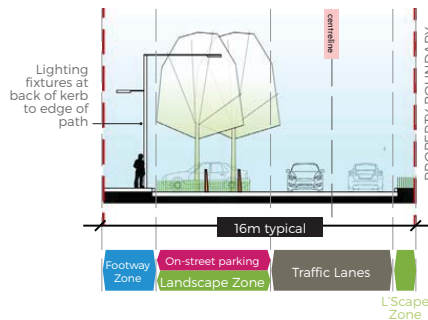


Figure 21: Mid-Block Laneway Typical Section H

Mid-Block Laneway | Softscape

Trees and planting are complementary to the adjoining street typology.

It is desirable for the land-owner to provide automated, reticulated irrigation systems maintained by land-owner.

Mid-Block Laneway | Hardscape

Select pavements complementary to the connecting street or laneway typology.

Mid-Block Laneway | Furniture

Furniture	Locations
Seats	Acceptable.
Bike Racks	Acceptable.
Bollards	Bollards are incorporated into passively irrigated planting beds between car spaces, due to flush kerbs.

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5.10 TYPOLOGY I | SERVICE LANEWAY

The following design guidance applies to Service Laneways in *Mill Central*.

Service Laneways are a movement corridor for service, delivery and emergency vehicles. Pedestrian and vehicular traffic will be separated by passively irrigated planting beds with slotted kerbs. Footway zone included on only one side of the street which is the dominant path of travel with fewer driveway crossovers to avoid conflict.

Service laneways may be dedicated road reserves or located on publicly accessible private land.



Figure 22: Service Laneway Typical Plan

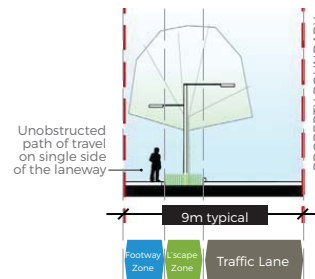


Figure 23: Service Laneway Typical Section I

Service Laneway | Softscape

Trees and planting are complementary to the adjoining street typology.

It is desirable for the land-owner to provide automated, reticulated irrigation systems maintained by land-owner.

Service Laneway | Hardscape

Select pavements complementary to the connecting street or laneway typology.

Service Laneway | Furniture

Furniture	Locations
Seats	Acceptable.
Bike Racks	Acceptable.
Bollards	Bollards are incorporated into passively irrigated planting beds, due to flush kerbs.

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5.11 TYPOLOGY J | SHARED LANEWAY

The following design guidance applies to Shared Laneways in *Mill Central*.

Shared Laneways are an active movement thoroughfare. They are corridors for public buses, private vehicles, bicycles and pedestrians with flush kerbs signifying a full trafficable surface. Depending on the width, pedestrian-exclusive (footway) zones may be delineated by planting beds.

Shared laneways may be dedicated road reserves or located on publicly accessible private land.

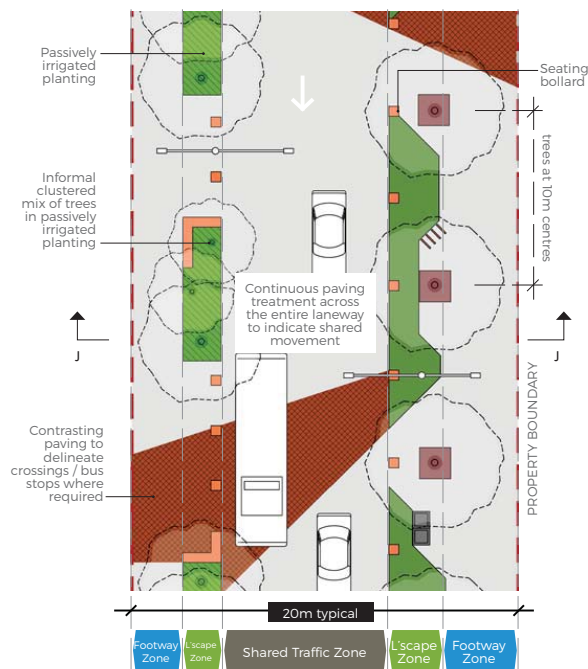


Figure 24: Shared Laneway Typical Plan

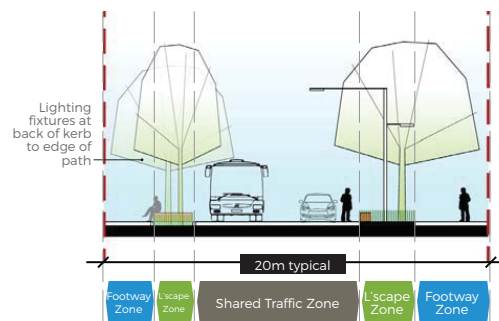


Figure 25: Shared Laneway Typical Section J

Shared Laneway | Softscape

Trees and planting are complementary to the adjoining street typology. The trees will shade 30% of the landscape and footway zones at full maturity.

It is desirable for the land-owner to provide automated, reticulated irrigation systems maintained by land-owner.

Shared Laneway | Hardscape

Select pavements complementary to the connecting street or laneway typology.

Shared Laneway | Furniture

Furniture	Locations
Seats	Acceptable.
Bike Racks	Acceptable.
Bins	Acceptable.
Bollards	In lieu of raised kerbs, seating bollards are used to indicate a separation between shared and pedestrian-exclusive zones.

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5.12 TYPOLOGY K | PEDESTRIAN LANEWAY

The following design guidance applies to Pedestrian Laneway in *Mill Central*.

Pedestrian Laneways are for pedestrian and cyclist use only and have no vehicular-traffic carrying capacity. The treatment appears as a continuous finish from boundary to boundary, with spatial definition provided by passively irrigated linear planting areas and seating walls. The lanes are shady and safe conduits for foot traffic with space for future temporary activation.

Pedestrian Laneways may be dedicated road reserves or located on publicly accessible private land.

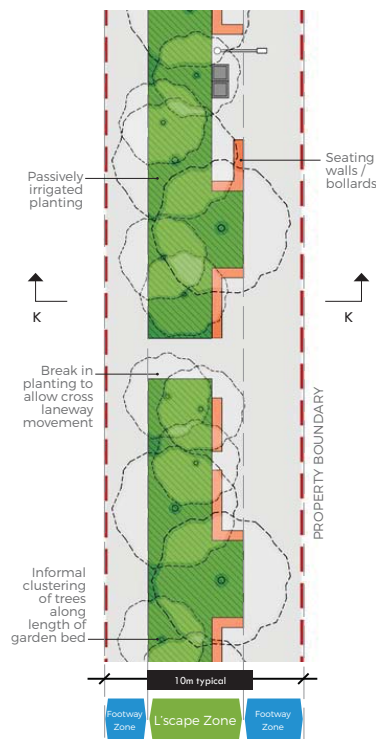


Figure 26: Pedestrian Laneway Typical Plan

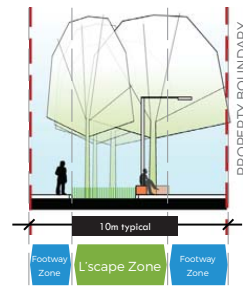


Figure 27: Pedestrian Laneway Typical Section K

Pedestrian Laneway | Softscape

Trees and planting are complementary to the adjoining street typology. The trees will shade 50% of the landscape and footway zones at full maturity.

It is desirable for the land-owner to provide automated, reticulated irrigation systems maintained by land-owner.

Pedestrian Laneway | Hardscape

Select pavements complementary to the connecting street or laneway typology.

Pedestrian Laneway | Furniture

Furniture	Locations
Seats	Acceptable.
Bike Racks	Acceptable.
Bins	Acceptable.

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6. AT-GRADE CAR PARK GUIDE



Figure 28: Parallel Car Park Typical Plan



Figure 29: Perpendicular Car Park Typical Plan (also applies to angular)

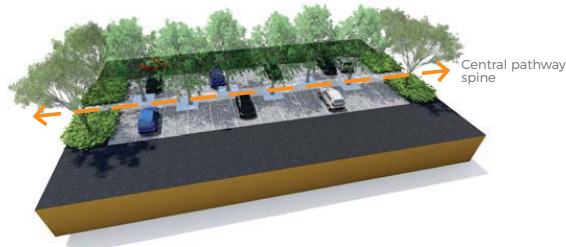


Figure 30: Perpendicular Car Park with median Typical Plan (also applies to angular)

At-grade Car Parking Attributes

Table 5: At-grade Car Parking Attributes

Flooding	<ul style="list-style-type: none"> ▪ Parking to be above 2% AEP (1 in 50 year ARI) event ▪ EV Charging to be above 1% AEP (1 in 100 year ARI) event
Smart Infrastructure	In all permanent at-grade car parks with more than 20 spaces, include: <ul style="list-style-type: none"> ▪ CCTV ▪ EV Charging and Parking Management
Lighting	Pedestrian and vehicular lighting
Power for events	In all permanent at-grade car parks with more than 100 spaces provide power for markets and events One power location provided for 100 car parks, plus one extra for every 50 additional car spaces A power location includes: <ul style="list-style-type: none"> ▪ 1x 10amp outlet ▪ 1x 15amp outlet
Hardscape: Car spaces	For at-grade car parks with more than 30 car spaces - minimum 50% of car spaces vehicle grade porous surfaces Impervious surfaces are acceptable for driveways and roads
Softscape	<ul style="list-style-type: none"> ▪ 50% of car spaces experience full or partial shade from a tree (at full maturity) (approximately 1 shade tree is provided for every 3 car spaces) ▪ No koala food trees
Pathway Network	<ul style="list-style-type: none"> ▪ Pedestrian pathways are provided adjacent to carpark/s ▪ Pedestrian crossings are provided to allow safe movement where appropriate ▪ Central pathway spine provided for car parks with more than 50 car spaces
Food Truck Space	Food truck spaces, with signage and linemarkings reserving the space are acceptable (refer Park typologies)
Wayfinding	Signage is provided in accordance with <i>Moreton Bay Regional Council Regional Wayfinding Style Guide</i>
Water Sensitive Urban Design (WSUD)	For at-grade car parks with more than 20 car spaces without any roof: <ul style="list-style-type: none"> ▪ integrated gardens are provided that are maximum 20m² each and no more than 500mm deep, and are planted to look similar to gardens; and ▪ linear planted swales are provided

For guidance on driveway design refer to section 5. **Guide and Cross Sections**, for the following typologies:

- Typology H - Mid-block laneway
- Typology I - Service Laneway
- Typology J - Shared Laneway

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Artist's Impression 4: Shared Street Typology - slow speed vehicular environment allowing pedestrians to cross

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7. GLOSSARY

Unless defined below, or in the *Economic Development Act 2012*, or the *Development Scheme*, the *MBRC Planning Scheme* definitions apply to this guideline.

Development Scheme	The Mill at Moreton Bay Priority Development Area - Development Scheme (August 2017) - https://www.dsdmip.qld.gov.au/edq/the-mill-at-moreton-bay.html
Landmark	an object or feature within a street, park or development that is easily seen and recognised from a distance. Landmarks aid wayfinding, enabling someone to establish their location.
Mill Central	A Place within the <i>Development Scheme</i> . Refer to Map 2 and section 2.6.1 Place 1 - Mill Central.
Mill Green	A Place within the <i>Development Scheme</i> . Refer to Map 2 and section 2.6.5 Place 5 - Mill Green.

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July 2019



SUPPORTING INFORMATION

Ref: [A18955796](#)

The following list of supporting information is provided for:

ITEM 3.1

MONTHLY REPORTING PACKAGE - JULY 2019 - REGIONAL

#1 Monthly Financial Reporting Package - July 2019

ITEM 3.1 MONTHLY REPORTING PACKAGE - JULY 2019 - REGIONAL - A18954976 (Cont.)

#1 Monthly Financial Reporting Package - July 2019

Moreton Bay Regional Council

Monthly Financial Report

Year to date result as at: 31 July 2019

Contents	Page No
Financial Statements	1 to 3
Statement of Sources and Application of Capital Funding	4
Segment Analysis	5
Commentary on Financial Results and Graphs	6 to 9
Treasury Report	10

Moreton Bay Regional Council

GENERAL MEETING
20 August 2019

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Supporting Information

ITEM 3.1 MONTHLY REPORTING PACKAGE - JULY 2019 - REGIONAL - A18954976 (Cont.)

Moreton Bay Regional Council STATEMENT OF COMPREHENSIVE INCOME For the period ended 31 July 2019

8% of the year
elapsed

	2019/20 Original Budget \$'000	2019/20 Amended Budget \$'000	2019/20 YTD Actuals \$'000	Actuals to Revised Budget 2019/20
Revenue				
Operating Revenue				
Rates and utility charges	313,151	313,151	79,784	25.48%
Fees and charges	37,272	37,272	2,646	7.10%
Grants, subsidies and contributions	20,149	20,149	1,453	7.21%
Interest revenue	45,426	45,426	3,513	7.73%
Other revenue	39,452	39,452	933	2.36%
Share of profit of associate - Operating Cash	20,000	20,000	1,667	8.33%
Total Operating Revenue	475,449	475,449	89,995	18.93%
Expenses				
Operating Expenses				
Employee benefits	(140,406)	(140,406)	(8,569)	6.10%
Materials and services	(190,865)	(190,865)	(10,789)	5.65%
Depreciation and amortisation	(97,721)	(97,721)	(8,143)	8.33%
Finance costs	(22,409)	(22,409)	(1,818)	8.11%
Total Operating Expenses	(451,401)	(451,401)	(29,319)	6.50%
Operating Result	24,047	24,047	60,676	252.32%
Share of Profit of Associate - Capital Non-cash	52,000	52,000	4,333	8.33%
Capital Revenue	96,868	96,868	5,731	5.92%
Capital Expenses	-	-	-	No Budget
NET RESULT	172,916	172,916	70,740	40.91%
Other Comprehensive Income				
Items that will not be reclassified to net result				
Increase/(decrease) in asset revaluation surplus	-	-	-	No Budget
Changes in the fair value of financial assets at fair value through other comprehensive income	-	-	(1,097)	No Budget
Total other comprehensive income for the year	-	-	(1,097)	No Budget
TOTAL COMPREHENSIVE INCOME FOR THE YEAR	172,916	172,916	69,643	40.28%

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ITEM 3.1 MONTHLY REPORTING PACKAGE - JULY 2019 - REGIONAL - A18954976 (Cont.)

Moreton Bay Regional Council STATEMENT OF FINANCIAL POSITION As at 31 July 2019

	2019/20 Amended Budget as at 30 June 2020 \$'000	2019/20 YTD Actual \$'000
Assets		
Current Assets		
Cash and cash equivalents	330,445	377,354
Trade and other receivables	48,848	73,715
Inventories	1,081	1,170
	380,374	452,239
Total Current Assets	380,374	452,239
Non-Current Assets		
Trade and other receivables	677,576	677,544
Investments	1,357,637	1,335,168
Property, plant and equipment	4,837,717	4,851,096
Total Non-Current Assets	6,872,930	6,863,808
Total Assets	7,253,304	7,316,047
Liabilities		
Current Liabilities		
Trade and other payables	44,918	29,196
Borrowings	37,334	34,685
Provisions	12,888	14,389
Other	21,427	13,676
Total Current Liabilities	116,567	91,946
Non-Current Liabilities		
Trade and other payables	-	-
Borrowings	332,801	344,307
Provisions	43,841	58,870
Total Non-Current Liabilities	376,642	403,177
Total Liabilities	493,209	495,123
NET COMMUNITY ASSETS	6,760,095	6,820,924
Community Equity		
Retained surplus	5,889,554	5,884,167
Asset revaluation surplus	870,541	936,757
TOTAL COMMUNITY EQUITY	6,760,095	6,820,924

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ITEM 3.1 MONTHLY REPORTING PACKAGE - JULY 2019 - REGIONAL - A18954976 (Cont.)

Moreton Bay Regional Council STATEMENT OF CASH FLOWS For the period ended 31 July 2019

	2019/20 Amended Budget as at 30 June 2019 \$'000	2019/20 YTD Actuals \$'000
Cash flows from operating activities		
Receipts from customers	413,708	50,596
Payments to suppliers and employees	(341,034)	(45,237)
Interest received	45,426	9,268
Non capital grants and contributions	20,567	1,453
Borrowing costs	(20,379)	(1,768)
Net cash inflow/(outflow) from operating activities	118,288	14,311
Cash flows from investing activities		
Payments for property, plant and equipment	(227,389)	(4,132)
Proceeds from sale of property, plant and equipment	21,800	144
Grants, subsidies and contributions	56,868	5,587
Net cash inflow/(outflow) from investing activities	(148,721)	1,599
Cash flows from financing activities		
Proceeds from borrowings	25,000	-
Repayment of borrowings	(33,916)	-
Net cash inflow/(outflow) from financing activities	(8,916)	-
Net increase/(decrease) in cash held	(39,349)	15,910
Cash and cash equivalents at the beginning of the financial year	369,794	361,444
Cash and cash equivalents at the end of the period	330,445	377,354

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ITEM 3.1 MONTHLY REPORTING PACKAGE - JULY 2019 - REGIONAL - A18954976 (Cont.)

Moreton Bay Regional Council STATEMENT OF SOURCES AND APPLICATIONS OF CAPITAL FUNDING For the period ended 31 July 2019

	Original Budget 2019/20 \$'000	Amended Budget 2019/20 \$'000	YTD Actuals 2019/20 \$'000
<u>Capital Funding Sources</u>			
Cash Utilised	202,936	202,936	3,654
Capital Grants and Subsidies received	33,368	33,368	478
Contributed Assets and assets not previously recognised	40,000	40,000	-
Loans received	25,000	25,000	-
Total Capital Funding Sources	301,305	301,305	4,132
<u>Capital Funding Applications</u>			
Capital Expenditure	227,389	227,389	4,132
Contributed Assets and assets not previously recognised	40,000	40,000	-
Loan Redemption	33,916	33,916	-
Total Capital Funding Applications	301,305	301,305	4,132

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ITEM 3.1 MONTHLY REPORTING PACKAGE - JULY 2019 - REGIONAL - A18954976 (Cont.)

Analysis of Results by Segment

For the period ended 31 July 2019						
Operational Plan	Operating Revenue \$'000	Operating Expenses \$'000	Operating Result \$'000	Capital Revenue \$'000	Capital Expenses \$'000	Net Result \$'000
Engineering, Construction & Maintenance	12,521	(6,186)	6,335	622	-	6,956
Community & Environmental Services	2,198	(2,989)	(791)	-	-	(791)
Governance & Corporate Services	74,350	(19,393)	54,957	9,442	-	64,399
Planning	927	(751)	176	-	-	176
Total Council	89,995	(29,319)	60,676	10,064	-	70,740

Analysis of Results by Entity

For the period ended 31 July 2019						
Entity	Operating Revenue \$'000	Operating Expenses \$'000	Operating Result \$'000	Capital Revenue \$'000	Capital Expenses \$'000	Net Result \$'000
General	77,933	(28,175)	49,758	9,989	-	59,747
Waste	12,062	(1,144)	10,918	75	-	10,993
Total Council	89,995	(29,319)	60,676	10,064	-	70,740

The Performance at a Glance **as at 31 July 2019**

Synopsis

- * 8% of the financial year is complete.
- * The operating surplus is \$60.68 million.

Operating Revenue

- * Rates and Utility Charges are slightly above budget following the first quarterly rates levy.
- * User Fees and Charges and Operating Grants and Subsidies are aligning closely to budget at this time of year.
- * Other revenue is under budget after the first month as it is driven by tax payments received by Unitywater, which are received in arrears, hence July's payment is received in August.

Operating Expenditure

- * Employee Expenses and Materials and Services are below budget, predominately due to higher than expected levels of vacant positions.
- * Depreciation and Finance Costs are tracking to budget at this time of the year.

Capital Revenue

- * Infrastructure cash contributions are tracking above the target.
- * Capital grants and subsidies is well under target but this is all dependent on when work is completed so that Council can claim the grant. As very little capital work has been completed no grants can be claimed. Generally most grants are claimed in the second half of the financial year.

Capital Expenditure

- * For the year \$3.53 million has been spent on capital works, (which represents 2.24% of the capital program). This excludes the University project costs.

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ITEM 3.1 MONTHLY REPORTING PACKAGE - JULY 2019 - REGIONAL - A18954976 (Cont.)

Moreton Bay Regional Council

Comparative Table 2017/18 and 2018/19 to 2019/20*

Year to date result as at: 31 July 2019

8% of the year elapsed

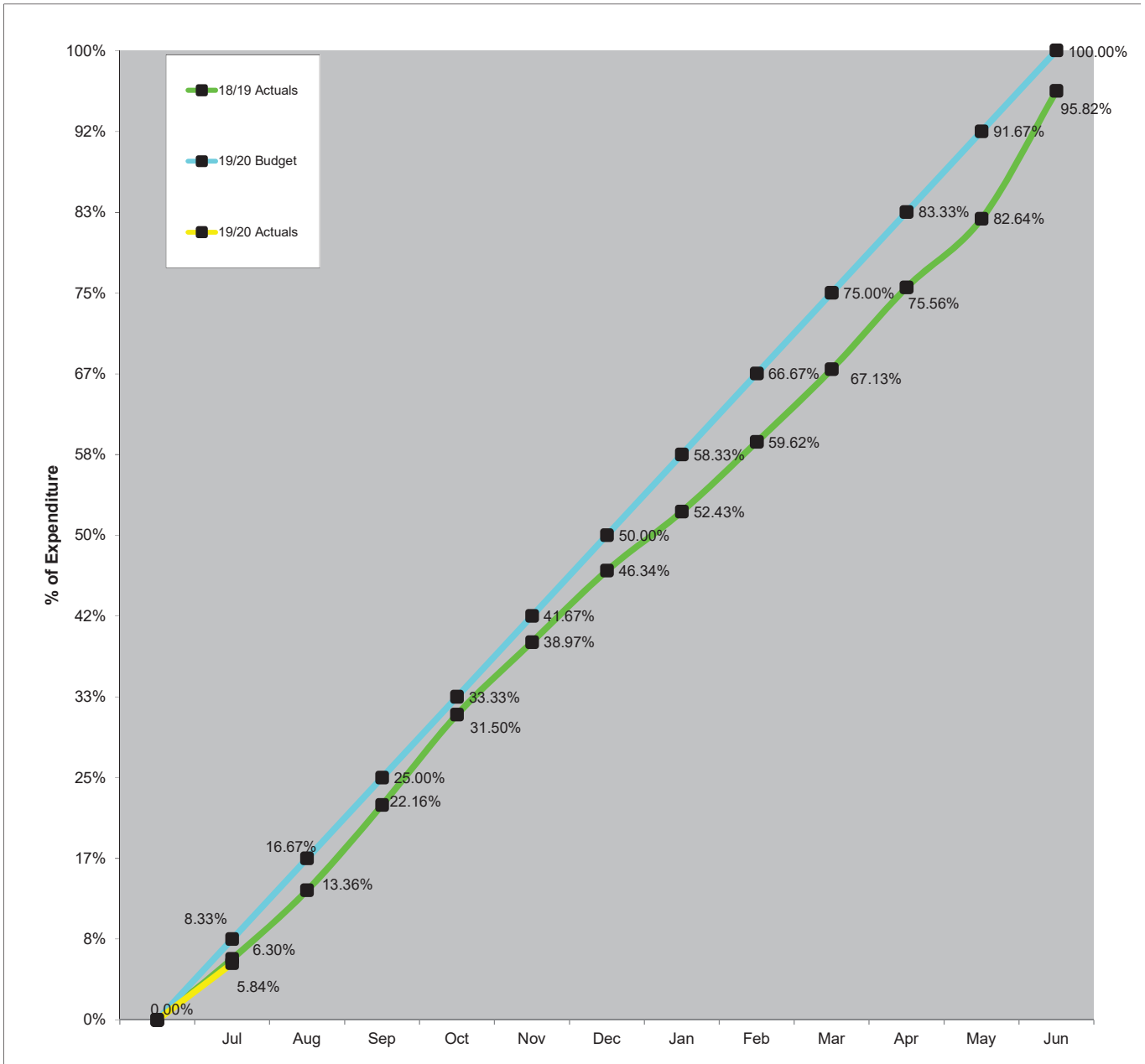
	Amended Budget 2019/20 \$'000	YTD Actuals 2019/20 \$'000	Actuals to Amended Budget 2019/20 %	Actuals to Amended Budget 2018/19 %	Actuals to Amended Budget 2017/18 %	Comments
Operating Revenue						
Rates & Utility Charges	313,151	79,784	25%	24%	24%	Revenue is slightly above target after the first quarter rates levy.
User Fees & Charges	37,272	2,646	7%	7%	7%	Revenue is tracking closely to budget and is comparable to the previous year.
Interest Revenue	45,426	3,513	8%	8%	8%	The % for 2019/20 is tracking to budget and is consistent with previous years.
Operating Expenses						
Employee Expenses & Material and Services	331,271	19,358	6%	6%	6%	Expenditure is tracking below budget but is comparable to the previous years.
External Loan Interest Expense	20,379	1,768	9%	9%	9%	Expenditure is tracking slightly above budget and is comparable to previous years.
Capital Revenues						
Infrastructure Cash Contributions	23,500	5,109	22%	15%	25%	Infrastructure cash contributions are tracking above the previous year and exceeding budget.
Contributed Assets	40,000	-	0%	0%	0%	No contributed assets have been recognised to date.
Grants & Subsidies	33,368	478	1%	1%	1%	The % of grants and subsidies received is tracking below budget and is comparable with previous years.
Capital Expenditure						
Total Capital Expenditure**	157,389	3,529	2%	1%	1%	Capital expenditure remains behind budget but comparable with previous years.
<p>* The data presented reflects the position of Council as at 31 July 2019 compared to the position of Council as at 31 July 2018 and 31 July 2017.</p> <p>** Capital Expenditure excludes the University Projects.</p> <p>The table focuses on key items of revenue and expenses across the comparative period and is useful guide in understanding what may have changed with regard to</p>						

ITEM 3.1 MONTHLY REPORTING PACKAGE - JULY 2019 - REGIONAL - A18954976 (Cont.)

Operating Expenditure

This graph compares the major components of operating expenditure (being employee expenses plus materials and services) on a percentage expended basis for the 2019/20 and 2018/19 years. The graph includes continuing service delivery expenses and operating initiative expenses.

The budgeted expenditure trend is set at 8.33% for each month. The cumulative actual expenditure trend for each month is graphed alongside the budget expenditure.

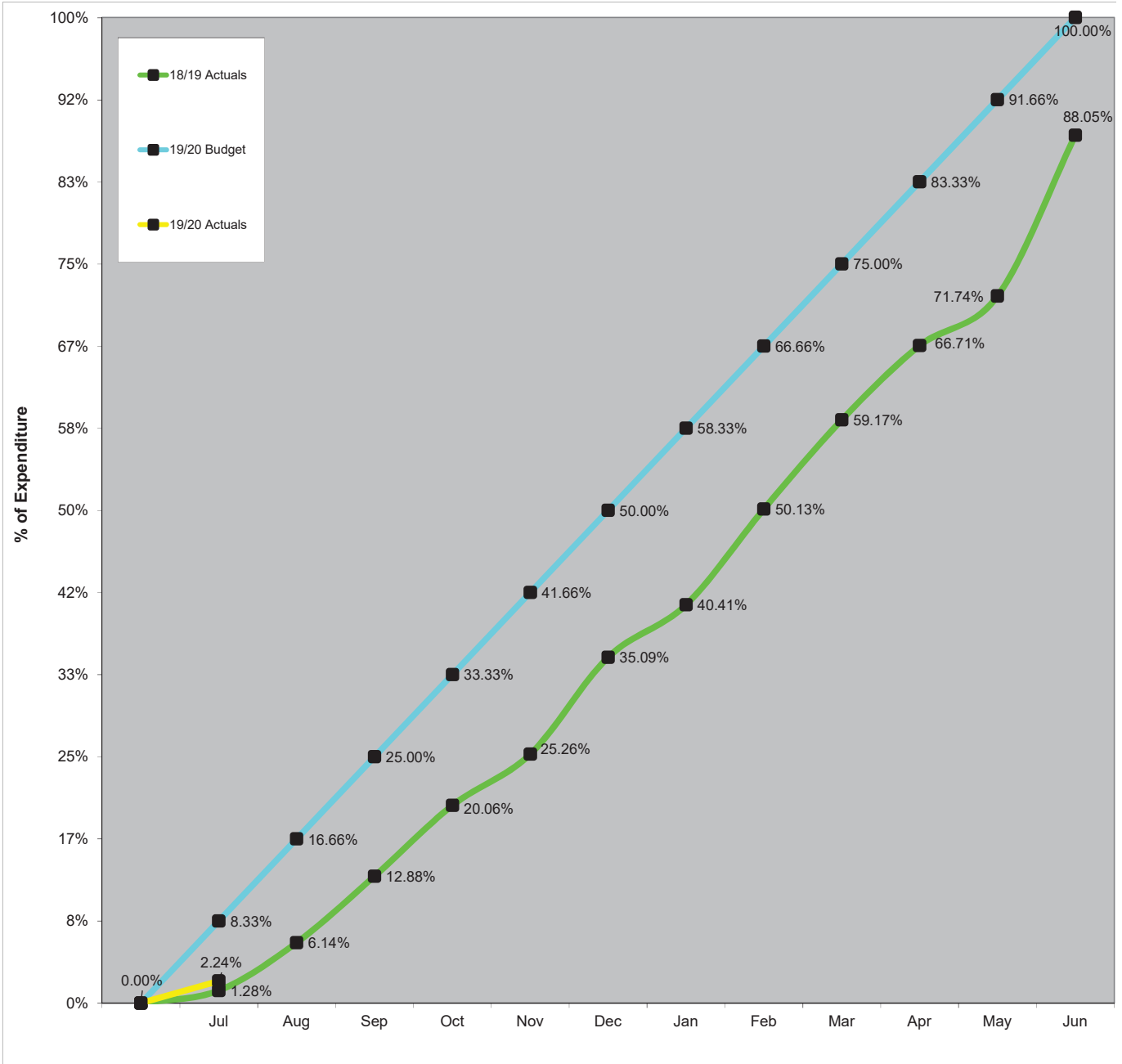


	Annual Amended Budget \$'000	Actuals to July \$'000	Actuals to Budget % spent
2018/19	297,446	18,725	6.30%
2019/20	331,271	19,358	5.84%

ITEM 3.1 MONTHLY REPORTING PACKAGE - JULY 2019 - REGIONAL - A18954976 (Cont.)

Capital Expenditure

This graph compares the capital percentage expended for the 2019/20 and 2018/19 years. The budgeted expenditure trend is set at 8.33% for each month. The cumulative actual expenditure trend for each month is graphed alongside the budget expenditure. The graph excludes capital expenditure associated with the University Project.



	Annual Amended Budget \$'000	Actuals to July \$'000	Actuals to Budget % spent
2018/19	185,827	2,370	1.28%
2019/20	157,389	3,529	2.24%

Moreton Bay Regional Council

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ITEM 3.1 MONTHLY REPORTING PACKAGE - JULY 2019 - REGIONAL - A18954976 (Cont.)

TREASURY REPORT

The Treasury Report highlights key areas of performance and compliance relating to Council's cash, investments and borrowings.

Investments

At 31 July 2019 all of Council's investments are in accordance with the approved Investment Policy. As at this date Council held 66.79% of funds outside of the Queensland Treasury Corporation (QTC). Term Deposit rates have increased slightly in recent times, providing better returns.

Investment Portfolio - Summary of Cash and Investments Held

Current Cash Investments					
Short Term Rating	Institution	Return	Term	Cash and Investment balance as at 31 July 2019	Interest earned YTD
A1+	Qld Treasury Corp*	1.99%	Short Term (45 days)	125,307,272	211,150
A1+	ANZ	1.47%	At Call	11,470,795	15,617
A1+	National Australia Bank	1.5%	At Call	45,025,560	33,976
A1+	ANZ	2.02% to 2.7%	124 to 364 days	40,550,035	76,320
A1+	Bankwest	2.65%	182 days	20,000,000	45,014
A1+	National Australia Bank	2.75% to 2.77%	357 to 363 days	30,000,000	70,238
A1+	Westpac	2.65% to 2.77%	364 to 365 days	60,000,000	139,033
A1	Suncorp				21,781
A2	Bank of Queensland	2.7% to 2.75%	182 to 279 days	30,000,000	69,644
A2	IMB	2.65%	363 days	5,000,000	11,253
A2	AMP Bank	2.8%	183 to 272 days	10,000,000	23,781
	Petrie Paper Mill Site Funds Trust Investments				16,708
					17,975
				*	
				377,353,661	752,490

* The QTC rate presented is the annualised interest rate for the month as provided by the Queensland Treasury Corporation.

Council has achieved a weighted average interest rate on all cash held of 2.53% pa in 2019/20.

Non-Current Investments				
Institution	Product	Term	Invested Value \$'000	Realisable Value \$'000
Queensland Investment Corporation	QIC Growth Fund	Greater than 5 years	100,000	107,431

Performance to Budget - Year to Date (YTD) Summary

8% of the year has elapsed

	Original Budget \$'000	Amended Budget \$'000	Actual YTD \$'000	Actual % Achieved	Comments
Interest Revenue on Investments	10,391	10,391	752	7%	Tracking as per Budget
Interest on Debt held in Unitywater	34,000	34,000	2,742	8%	Tracking as per Budget
Total Investment Income	44,391	44,391	3,494	8%	

Borrowings

Debt Position	\$ '000
Debt held as at 1 July 2019	378,992
New borrowings	0
Borrowings repaid	0
Debt held as at 31 July 2019	378,992

As at 31 July 2019 the weighted average interest rate of all Council debt is 5.63%

SUPPORTING INFORMATION

Ref: [A18787911](#)

The following list of supporting information is provided for:

ITEM 5.1

NEW LEASE - DELANEY'S CREEK PONY CLUB INC - DIVISION 12

#1 Delaney's Creek Pony Club Inc - Proposed lease area

ITEM 5.1 NEW LEASE - DELANEY'S CREEK PONY CLUB INC (Cont.)

#1 Delaney's Creek Pony Club Inc - Proposed lease area

